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# Design Guidelines Manual

City of Hallandale Beach, Florida  
City of Choice



Prepared for:

# **City of Hallandale Beach, Florida**

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Adopted: March 30, 2006



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## introduction

Design guidelines are intended to implement and provide guidance on site and building design throughout the City of Hallandale Beach. The purpose of this document is to provide direction to land developers in preparing plans and to the Planning and Zoning Division in its review of development projects. It is important to remember that all projects are also subject to the required reviews of each agency.

The primary intent of these guidelines is to establish and promote standards for development planning and urban design.

The standards and guidelines provide direction as to how private development should relate to framework of public amenities in a way that will serve the long term vision as well as accommodate immediate opportunities. They have been conceived to allow flexibility within the parameters of a clearly defined and supported vision that will provide lasting benefit to the citizens of Hallandale Beach.

The vision of high quality public amenities and superior private development are interdependent and mutually reinforcing in achieving the ultimate vision for the city.

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## general standards and design guidelines

### • **Intent of Hallandale Beach Design Standards**

To utilize developed public spaces, such as streets, park and parkways, to organize and coordinate development.

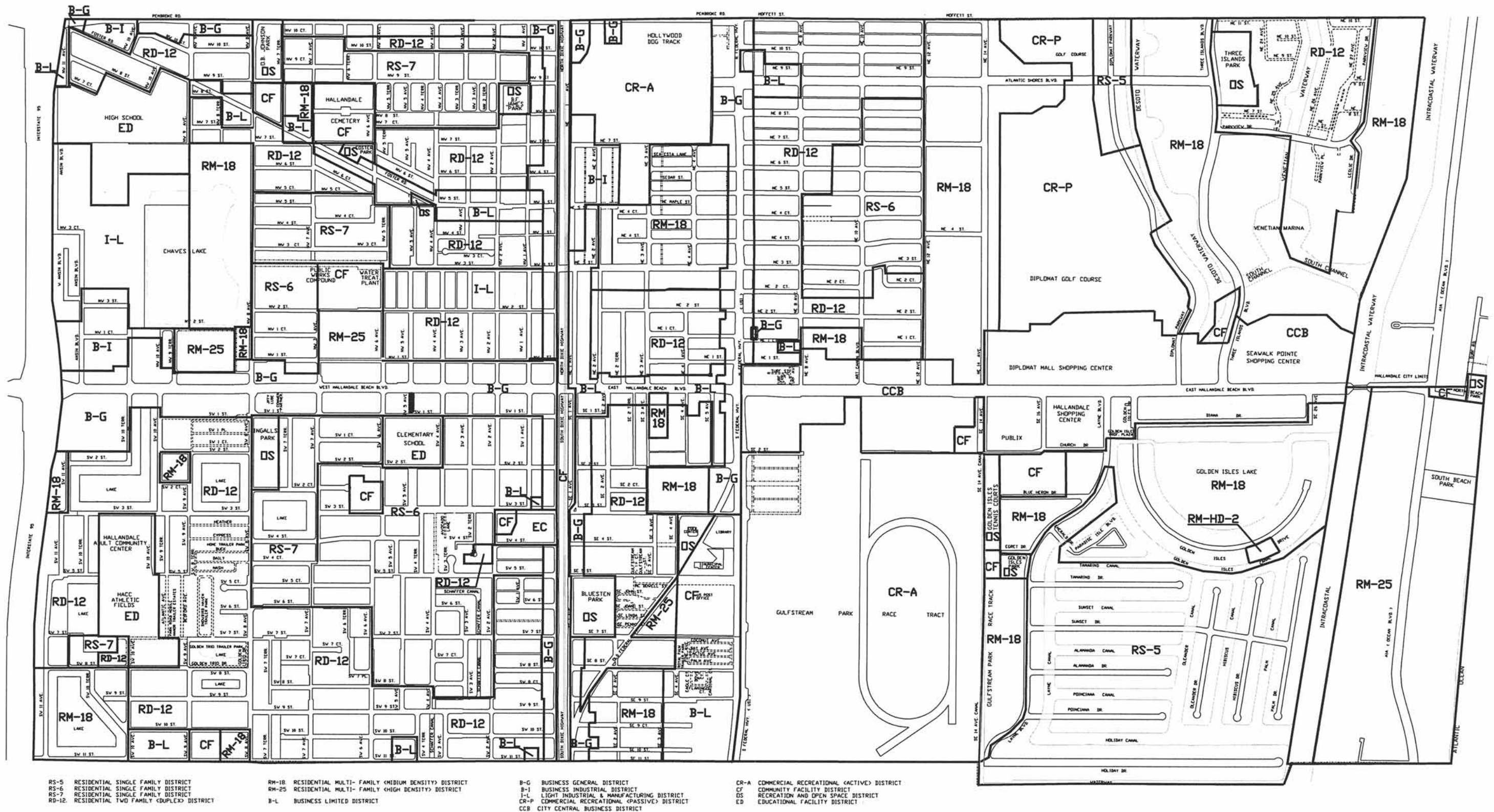
To accommodate a broad mix of development types that encourage alternate transportation, especially walking, and transit use.

To construct the early phases of development in a manner that establishes a pattern and character for the long-term evolution of the city.

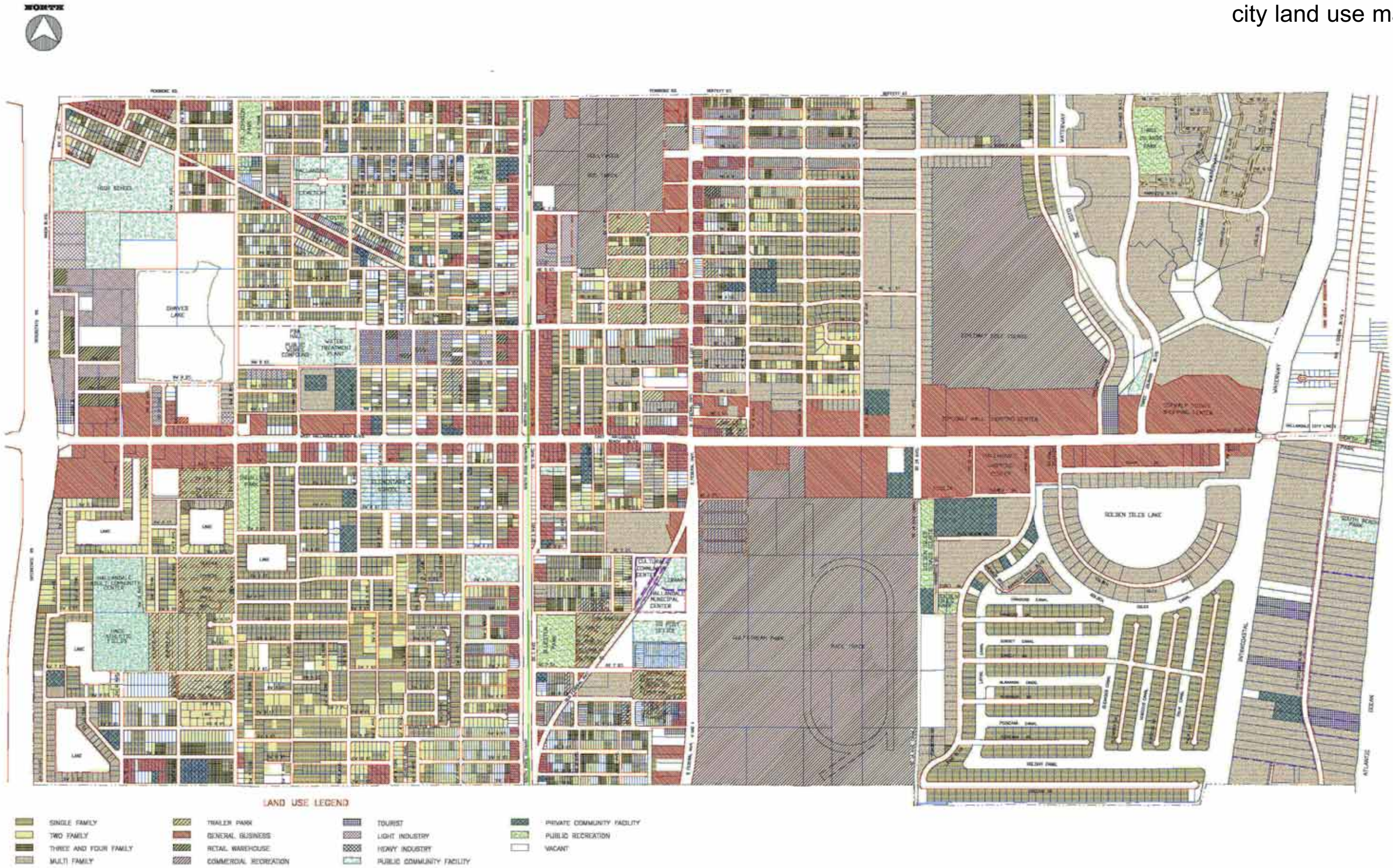
To develop a built environment that provides diversity, configuration and scale of development that promotes pedestrian activity, a sense of place and community.

The following Intent Statements and Guidelines have been adopted to provide specific direction to the broad design goals defined above.











public streetscape

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## public streetscape

### 1.0 Building and Site Design

The general requirements outlined in this chapter shall serve to supplement the minimum aesthetic and design standards for all site development, buildings, structures, alterations or additions.

In addition, the City of Hallandale Beach Master Design and Redevelopment Plan for Hallandale Beach Boulevard and North Federal Highway prepared by the consulting firm of Siemon and Larsen shall apply as referenced herein.

It is required that all site development or redevelopment show proper design concepts consistent with the standards outlined in these guidelines.

#### **Building Design**

Buildings or structures, which are a part of an existing or future group of buildings, shall have a unity of character and design. The relationship of forms and the use, texture, and color of materials shall be such as to create a harmonious whole site. Harmony can be achieved through the proper consideration of scale, proportions, site planning, landscaping, materials and color.

Buildings and site design relationships shall conform to the following criteria:

- a. Buildings or structures located along strips of land or on single sites and not part of a unified multi-building complex shall strive to achieve visual harmony with the surroundings.
- b. Retail or office establishments, which are located on corners, are recommended to place windows on each wall that faces a street, parking area or driveway.



*"Similarity of style and color"*



*"Unity of character and design"*



*"Corner entrys enhance intersection"*

## public streetscape

- c. In the case of buildings with multiple storefronts and shopping centers with outparcel development, façade treatment shall be coordinated. Such façade treatments include: building colors, windows, storefronts, signage and awnings.
- d. All vending machines, any facility dispensing merchandise, or a service on private property shall be confined to a space built into the building or buildings, or enclosed in a separate structure compatible with the main building.
- e. When garage structures are provided in a commercial or mixed-use development, such shall be designed to incorporate a retail or residential design into the structures facade (wraps) at ground level.
- f. Storefronts shall have easily identifiable entrances.
- g. "Take out" or "pick up" windows for retail or other establishments shall not be located on a building façade that faces a public right-of-way, unless they are designed in such a manner as to be an aesthetic asset to the building and neighborhood.
- h. Roof-mounted mechanical equipment and elevator shafts shall be screened by a parapet wall or grills and shall be painted in muted colors or match the building and shall not be visible from the street.
- i. Place ground-mounted air conditioning units and other mechanical equipment to the rear of the building, screened from public and on-site pedestrian view.



*"Coordinated facade treatments"*



*"Garage structure with retail/residential design"*



*"Grid treatment designed to screen parking levels."*



## public streetscape

- j. Pre-fabricated homes are prohibited in new construction.
- k. Fences shall be made of wrought iron or aluminum bars with intermittent posts. Masonry walls are also permitted, with forty (40) percent of the wall opaque. Chain link fences and privacy wood fences are prohibited along the Corridors. Sharp projections, barbed wire or other hazardous materials are not permitted as any part of a fence or wall. Wrought iron and aluminum bar fences shall either be black, white or match the color of the building. Masonry walls shall match the building color or reflect Florida coastal themes. Color shall be muted tones such as soft yellow, cream, granite and rose.

### Site Design Relationships

The coordination of façade components helps establish an identity for an office building, industrial building or shopping plaza. Therefore, for all unified developments and shopping centers including principal buildings and out parcel development, all buildings and signage shall demonstrate compatibility in materials and consistency in style throughout all exterior elevations. The following standards shall be applied to all new development as well as substantial redevelopment. Buildings and signage shall demonstrate the following:

- a. Compatibility with adjacent land uses in terms of scale and lot coverage.
- b. Utilize color schemes that blend with those of neighboring developments as well as consistency in color schemes for the site. Accent colors and materials shall be chosen to enhance architectural details.
- c. In the case of buildings with multiple storefronts and shopping centers with out parcel development, façade treatment shall be coordinated and have like details. Such façade treatments include: building colors, windows, floor, storefronts, signage, awnings, roof materials and roof pitch.
- d. Building signs shall be designed as integral architectural elements with proportions related to the surfaces to which they are attached.



*"Iron rail fence divides uses"*



*"Consistent color and style"*



*"Coordinated facade features"*

## public streetscape

### 1.1 Color Palette and Architectural Theme

The City of Hallandale Beach has established building color criteria for all buildings and accessory structures. The design of these guidelines is to enhance the appearance of development within the city, which will improve and/or maintain property values for all city residents and property owners.

The city will be reviewing building paint colors for all residential, commercial, industrial and other non-residential buildings. Owners, developers and/or contractors will be required to receive approval from the city for the base, secondary and trim colors before painting these areas.

The charge to the applicant for this review is \$25.00, which applies to single-family, duplexes, and mobile homes. Multi-family units, non-residential, commercial and industrial developments will be charged per the City's fee schedule.

Prior to purchasing paint for your home or business, please submit paint samples to the City for approval.

Colors for buildings are expressed in three categories:

1. Base building colors – primary building walls
2. Secondary building colors - larger "trim" areas such as a lower building base, building design details, or accent trim around windows and doors.
3. Trim colors - small area of color such as decorative trim along rooflines, and areas around windows, doors, frames and signage.

Note: Single-family buildings, duplexes and mobile homes are exempt from color approval if base, secondary and trim colors are to be painted white and/or off-white.

Requirements for Color Palette Submission:

- Property Address
- Location Map
- Provide 6"x6" Color Samples
- Clearly Identify Base, Secondary and Trim colors
- Photo or Elevation of the Structure

A color palette is available at the City Development Services Department for review of approved colors.



## Residential

### Base Building Colors

This color category relates to the main area of walls on the building. Recommended colors include whites through soft pastel colors such as grays, pinks, yellows, greens and blues. Other colors may include deeper but muted indigenous earthen or indigenous colors such as beiges, clays, light golds, and terracottas. Day-Glo colors are prohibited.

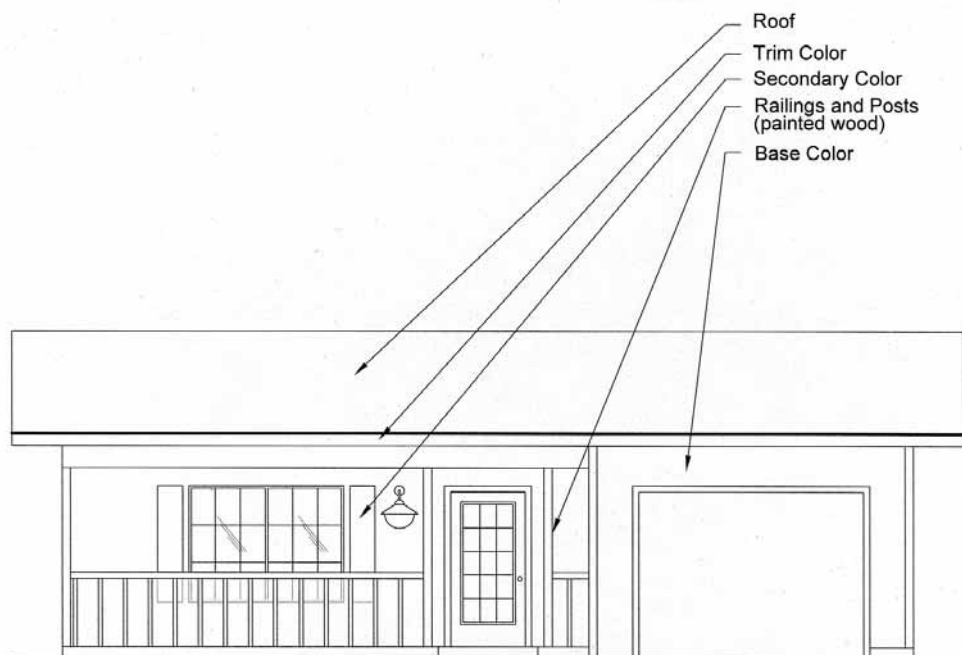
### Secondary Building Colors

These colors are used for larger trim areas of the building and are limited to mid-range intensity of the base building or complementary color. Secondary colors shall be limited to 25% of the major surface plane they are used upon.

### Trim Colors

Trim colors are used for accent purposes and are the most intensive group of colors allowed. They shall be limited to not more than 5% of the building surface. These trim colors are usually darker and more intense than other colors. Dark blues, greens and even reds are appropriate. Light colors for trim, including whites and beiges are encouraged.

Single-family buildings, duplexes and mobile homes are exempt from color approval if base, secondary colors are to be painted white and/or off-white.



## Commercial

Color should be chosen to add to the retail environment of these buildings. More latitude will be given to retail color use than is given to other buildings within the City (residential, industrial). The use of color to attract attention to a business (using the building or wall colors as a sign) from a distance is prohibited. Day-Glo colors are also prohibited.

### Base Building Colors

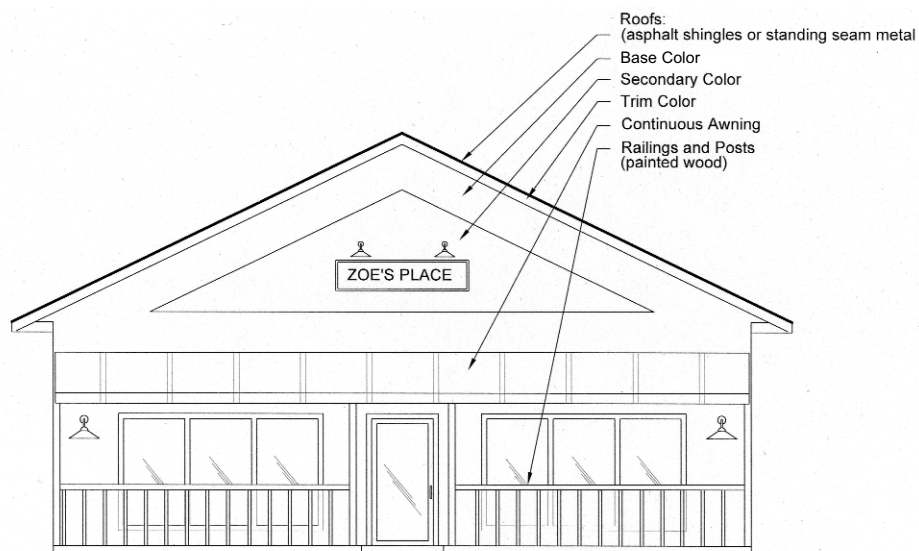
This color category relates to the main area of walls on the building. Recommended colors include whites through soft pastel colors such as grays, pinks, yellows, greens and blues. Other colors may include deeper but muted indigenous earthen or indigenous colors such as beiges, clays, light golds, and terra cottas.

### Secondary Building Colors

These colors are used for larger trim areas of the building and are limited to mid-range intensity of the base building or complementary color. Secondary colors shall be limited to 50% of the major surface plane they are used upon. Colors that are associated with a business identity also fall into this category provided they are not overly intensive. Base and secondary colors are interchangeable in proportion and hue.

### Trim Colors

Trim colors are used for accent purposes and are the most intensive group of colors allowed. In addition to business identifying colors, it is encouraged that trim colors be chosen from the "natural palette" of South Florida. These colors include greens, blues, yellows and others that are found in the regions lush landscape and natural features. These colors shall be limited to 5% of any single wall area.





## public streetscape

**Industrial**

Because of the larger size of warehouses and other industrial buildings, a neutral color for the main building mass is recommended. Day-Glo colors are prohibited.

**Base Building Colors**

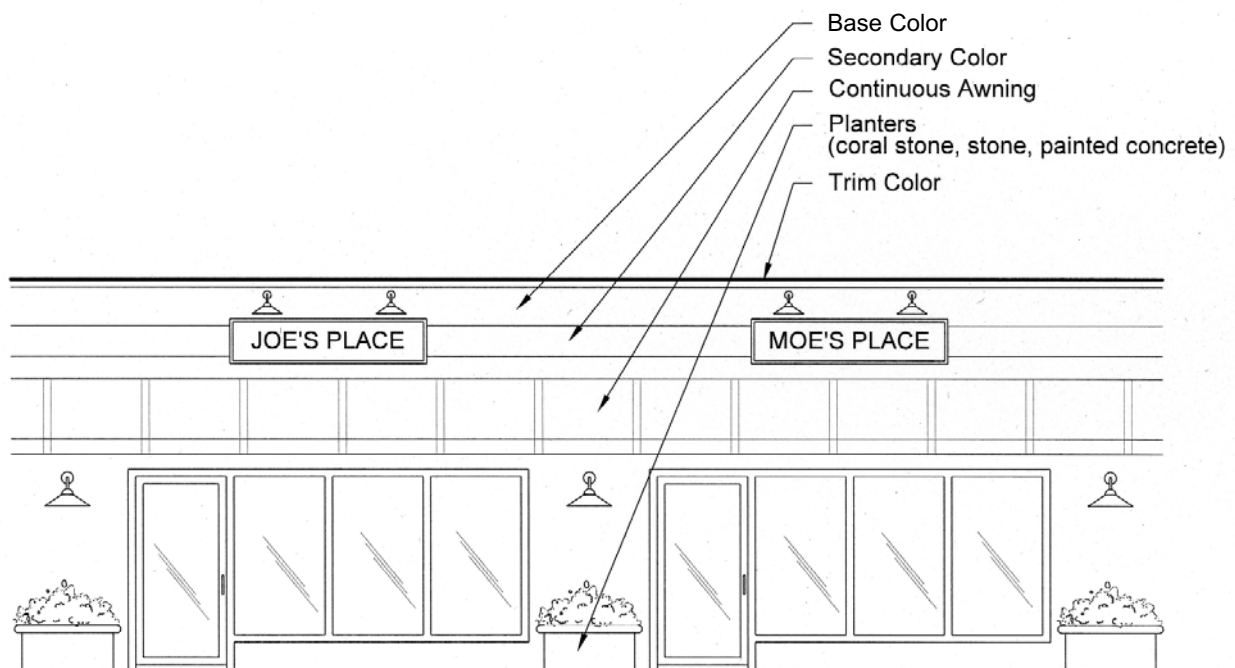
Whites, beiges, greys, and light yellows are encouraged. The use of colors to attract the attention of a business (using the building or wall colors as a sign) from a distance is prohibited.

**Secondary Building Colors**

In order to break up a large façade, a secondary building color (limited to 25% of the wall area) of medium intensity hues of the base building color, or one complimentary to it, is encouraged. Painting of the base elements, banding, framing and parapets with a secondary building color is also encouraged.

**Trim Colors**

Darker and more intense trim colors can add detail to the façade. Trim colors should be limited to building's details and signage.



## 1.2 Landscaping and Open Space

### Street Trees

Trees improve air quality, reduce storm water runoff, provide cooling effects for the urban heat island, increase property values and create urban wildlife habitat. They can also greatly increase the quality of life in a downtown setting. There is no question that trees are a valuable part of the city infrastructure and need to be managed as proactively as the streets and public utilities. The importance of healthy trees will increase as the density of downtown increases. Lack of adequate growing conditions for trees contributes to their ultimate decline. For purposes of developing a consistent landscape theme within the City, designated street trees and recommended landscape technique are provided in this section

- (1) Royal Palm Trees are the designated street trees on Hallandale Beach Boulevard, Federal Highway and S.R. A1A. The minimum height of trees at the time of planting is twenty (20) feet overall height; ten (10) feet of grey wood, street trees for other corridors shall be designated by the city manager or his designee.
- (2) Hedges shall be planted and maintained to form a continuous, solid screen as required in Section 32-385 Landscaping and General Requirements, in the Zoning and Land Development Code. All hedges shall be continuous and of consistent, approved plant material. If hedge material is replaced, the most dominant landscape species shall be the replacement material.

Requirements as follows:

- a. Trees shall be provided along major traffic and pedestrian corridors.
- b. Provide a minimum of 5' x 5' by 3.5' deep tree wells in existing or new sidewalks, provided there is a minimum 36" clear area in front of tree to permit passing in compliance with ADA requirements. Trees may also be located in islands created in the parking zone.
- c. Consider the use of continuous street tree trenches to provide maximum soil area for roots to spread, and water and air to penetrate.



*"Royal Palms on the boulevard"*



*"Shade trees enhance streetscape"*



## public streetscape

- d. Allow sufficient room for tree canopies to grow and develop without conflict with other building elements and overhead utilities.
- e. Install irrigation systems to provide adequate water to establish and maintain trees.
- f. In high pedestrian areas, install tree guards to protect the trunks from damage.
- g. Select trees that are adapted to the harsh conditions of a dense urban environment.
- h. Trees placed near overhead utility lines shall conform to FPL requirements in terms of clearance and recommended tree species.

### Landscaping

Landscaping in Hallandale Beach should compliment the old Florida/Maritime theme, using native plant materials and street furnishings that carry the theme, and provide continuity throughout the district. In addition, public safety will be a priority using the principles of CPTED to create high visibility areas and natural access control. To provide for a continuous attractive pedestrian and vehicular corridor in the Overlay Districts, applicants are encouraged to include the trees and shrubs they wish to plant from the approved list provided in this manual. With regards to landscape design for both new construction and existing buildings, the following should apply:

- a. Ornamental trees or palms should be placed in front of buildings in such a manner as to provide visual transparency. Shade trees and palms shall be used adjacent to open spaces and parking lots. Planting areas shall be designed with multi-layers of plant material including shrubs and ground covers.
- b. Landscaping should complement and enhance the overall architectural and design theme of the property, but not overpower it.
- c. Rhythm should be maintained along public streets through the uniform placement of trees.
- d. Blank walls greater than 25' in length and other unattractive areas of a site or building should be heavily screened with landscaping. Rooflines or storefronts of twenty-five (25) feet or greater shall be broken by vertical landscaping materials. Shade and accent trees planted at twenty (20) foot intervals shall be required to achieve this screening.



*"Grates protect trees in high traffic areas"*



*"Landscape to enhance architectural theme"*



*"Large walls broken up with landscape elements"*

### public streetscape

- e. Large parking areas and driveways shall be heavily landscaped along the perimeter and with interior and terminal islands.
- f. Landscape design should utilize the CPTED principles of natural surveillance, natural access control and territorial reinforcement.
- g. The use of native trees, shrubs and ground covers are encouraged to be incorporated into the landscaping around proposed developments. Local flora will be maintained as part of the built environment and the demand on our local water resources will be minimized.
- h. The placement and design of landscaping shall maximize visibility to provide natural surveillance.
- i. Landscape design shall incorporate with design of other physical features, such as sidewalk, pavements, lighting and fences to emphasize public entrances, define and reinforce ownership of property.
- j. Tree and palm heights and spread shall allow sufficient visibility, not completely block views of/from doors, windows and streets.
- k. Shrubs and ground cover shall be planted along public right-of-ways or around parking and public open areas.
- l. Landscape design will utilize principles of xeriscape landscaping, while retaining the tropical beach resort atmosphere.
- m. Landscape plans must be drawn, signed and sealed by a Florida Registered Architect or Landscape Architect.
- n. In addition to the design standards, all landscaping shall meet the standards of the City of Hallandale Beach Landscape Code.
- o. Landscaping in 15' line of site triangle at intersections of right of ways and at driveways shall conform to height clearances of bushes and trees, and maximum tree calipers in accordance with the Hallandale Beach zoning code.



*"Shade trees enhance streetscape"*



*"Grates protect trees in high traffic areas"*



## public streetscape

**Suggested Plant List (Groundcovers & Shrubs)**

Common Name	Botanical Name
<u>Groundcovers:</u>	
Asparagus Fern	Asparagus densiflorus
Aztec Grass	Liriope muscari "Variegata"
*Beach Sunflower	Helianthus debilis
Blue Daze	Evolvulus spp.
Boston/Sword Fern	Nephrolepis spp.
*Coontie	Zamia floridana
*Daylily	Hemerocallis spp.
Dwarf Yaupon	Ilex vomitoria "Nana Schellings"
Firecracker Plant	Russelia equisetiformis
Indian Hawthorn	Rhaphiolepis indica
Juniper	Juniperus spp.
*Lantana	Lantana spp.
Liriope	Liriope spp.
Mexican Bluebell	Ruellia brittoniana
Periwinkle	Catharanthus roseus
Sensitive Plant	Mimosa strizillosa "Nelo trope"
Spider Lily	Hymenocallis latifolia
Wart Fern	Polypodium scolopendria
Wedelia	Wedelia trilobata
Zephyr/Rain Lily	Zephyranthes spp.
<u>Shrubs:</u>	
Cardboard Palm	Zamia furfuracea
*Cocoplum	Chrysobalanus icaco
Crinum Lily	Crinum americanum
Dwarf Carissa	Carissa macrocarpa
Dwarf Pittosporum	Pittosporum tobira "wheeleri"
*Jamaica Caper	Capparis cynophallophora
Jasmine	Jasminum spp.
Lady of the Night	Brunfelsia americana
*Muhly Grass	Muhlenbergia spp.
*Myrsine	Myrsine guianensis
Orange Jasmine	Murraya paniculata
Plumbago	Plumbago auriculata
Tree Philodendron	Philodendron selloum
Walters Viburnum	Viburnum obovatum
*Wild Coffee	Psychotria nervosa

\* native plants

## public streetscape

**Suggested Plant List (Trees & Vines)**

Common Name	Botanical Name
<u>Small Trees:</u>	
Crepe Myrtle	Lagerstroemia indica
Dwarf Poinciana	Caesalpinnia spp.
Frangipani	Plumeria rubra
*Orange Geiger	Cordia sebestena
*Silver Buttonwood	Conocarpus erectus var. sericeus
*Stoppers	Eugenia spp.
Tree Jatropha	Jatropha spp.
Tree Ligustrum	Ligustrum spp.
White Geiger/Texas Olive	Cordia boissieri
Yellow Elder	Tecoma stans
<u>Medium Trees:</u>	
*Black Ironwood	Krugiodendron ferreum
*Dahoon Holly	Ilex cassine
Orchid Tree	Bauhinia spp.
*Pigeon Plum	Coccoloba diversifolia
*Pitch Apple	Clusia rosea
Pink Tabebuia	Tabebuia heterophylla
Sabicu	Lysiloma latisiliquae
*Seagrape	Coccoloba uvifera
Yellow Tabebuia	Tabebuia caraiba
<u>Large Trees:</u>	
Calophyllum/Beauty Leaf	Calophyllum spp.
*Green Buttonwood	Conocarpus erectus
*Gumbo Limbo	Bursera simaruba
*Live Oak	Quercus virginiana
*Paradise Tree	Simarouba glauca
*Queen's Crepe Myrtle	Lagerstroemia speciosa
*Satin Leaf	Chrysophyllum oliviforme
*West Indian Mahogany	Swietenia mahogani
*Wild Tamarind	Lysiloma bahamensis
<u>Vines:</u>	
Bougainvillea	Bougainvillea spectabilis
Creeping Fig	Ficus pumila
Yellow Allamanda	Allamanda cathartica

\* native plants



## public streetscape

**Suggested Plant List**

Common Name	Botanical Name	Credit Count
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Palms must meet minimum landscape requirements and shall be credited as follows:

Large Palms:

Alexander Palm	Ptychosperma elegans	2 to 1
*Cabbage/Sabal Palm	Sabal palmetto	3 to 1
Canary Island Date	Phoenix canariensis	1 to 1
Coconut Palm	Cocos nucifera	1 to 1
Foxtail Palm	Wodyetia bifurcatia	2 to 1
Medjool Date Palm	Phoenix dactylifera "Medjool"	1 to 1
*Queen Palm	Syagrus romanzoffiana	2 to 1
*Royal Palm	Roystonea regia	1 to 1
Washington Palm	Washingtonia robusta	3 to 1

\* native plants

**Prohibited and Non-Credit Species**

The City of Hallandale Code of Ordinances, refers to the types of trees which are exempt from provisions of Section 32. The following identifies trees and plants, which are either prohibited in the City of Hallandale Beach or non-credit species.

Prohibited Species

The following trees shall not be planted within the City Limits of Hallandale Beach:

Common Name	Botanical Name
African Tulip Tree	Spathodea campanulata
Australian Pine	Casurina equisetifolia/lepedophloia
Bischoffia	Bischoffia javanica
Brazilian Pepper/Florida Holly	Schinus terebinthifolius
Cajeput Tree	Melaleuca leucandendra
Carrotwood	Cupaniopsis anacardiodes
Chinese Tallow Tree	Sapium sebiferum
Downy Rose Myrtle	Phodomyrtus tomentosa
Earleaf Acacia	Acacia auriculaeformis
Eucalyptus	
Ficus Species	(permitted as a hedge material)
Guava	Psidium quajava
Indian Rosewood	Dalbergia sissoo
Java Plum	Syzygium cumini
Lead Tree	Leucaena leucocephala
Mahoe	Hibiscus viliaceus
Norfolk Island Pine	Araucaria heterophylla
Paperback Tree	Melaleuca quinquenervia
Poison Wood	Metopium toxiferum
Rose Apple	Syzygium jambos
Schefflera	Brassala actinophylla
Silk Oak	Grevillea robusta
Surinum Cherry	Eugenia Uniflora
Tropical Almond	Terminalia catappa
Women's Tongue	Albizia lebbeck



## public streetscape

Non-Credit Species

The following plants may be planted within the City Limits of Hallandale Beach, but shall not be considered for purposes of meeting the minimum requirements of Article IV, Landscaping Requirements of the Zoning and Land Development Code:

Common Name	Botanical Name
Sweet Acacia	Acacia

Non-Salt tolerant species planted east of the Intracoastal Waterway or 200 feet from a salt water body.

Non-Credit Palms

Blue Latania	Latania loddigesii
Chinese Fan Palm	Livistonia chinensis
European Fan Palm	Chamaerops humilis
Fiji Island Fan Palm	Pritchardia pacifica
Hurricane Palm	Dictyosperma album
Macarthur Palm	Ptychosperma macarthuri
Palmyra Palm	Borassus flabellifer
Pigmy Date Palm	Phoenix roebellini
Pindo Palm	Butia capitata
*Saw Palmetto	Seona repens
*Silver Palm	Coccothrynax argentata
Solitaire/Alexander	Ptychosperma elegans
Spindle Palm	Mascarena verchafee
Tailpot Palm	Corypha spa
*Thatch Palm	Thrynax spp.
Thurston Palm	Pritchardia thurstonii
Triangle Palm	Neodypsis decaryi
Windmill Palm	Trachycarpus fortunei

## public streetscape

### Plant Categories

Proposed landscape plans for development or redevelopment should incorporate the following information into the plant list and summary:

#### Salt Tolerance:

High	Plants are highly resistant to salt drift and can be used in exposed environments.
Moderate	Plants tolerate some salt spray, but grow best when protected by buildings, fences, or plantings of salt tolerant species.
Low	Plants have poor salt tolerance and always should be used well back of exposed areas and be protected by buildings, fences, or more salt tolerant species.
Not	Plants have extremely low to no salt tolerance and should not be used near exposed areas even if protected.

#### Light Requirements:

FS	Full Sun; these conditions are generally required for maximum growth and flowering and are met in southern locations in the landscape.
FS-PS	Full Sun – Partial Shade; plants within this category are adaptable to a range of light conditions. Full sun should be provided, but filtered sun through overhead canopy trees is adequate. Eastern, western and southern locations provide these conditions.
FS-DS	Full Sun – Dense Shade; plants that are quite adaptable to varied light conditions and will grow well in any location in the landscape.
PS-DS	Partial Shade – Dense Shade; plants that require shaded conditions for best growth. These conditions are provided under overhead canopy trees and in northern locations of the landscape.

#### Foliage:

E	Evergreen
D	Deciduous
SEV	Semi-Evergreen



*"Royals planted in repetition creates design impact"*



*"Coconut palms produce needed shade in full sun areas"*

## public streetscape

**Tree Relocation**

Before the City issues a tree removal permit that allows the replacement of any tree, the applicant must demonstrate that relocation is not a viable alternative. Relocation shall occur either within the site or off-site with the concurrence of the City where the site is public property. If any tree is to be relocated either on-site or off-site, a relocation plan shall be submitted. Relocation plans, as required by this Section, must first be reviewed and approved prior to granting any tree removal permit. Before a permit is issued for tree relocation, the posting of performance bonds may be required.

Methods for relocation – the following guidelines shall be utilized to ensure successful transplanting of trees:

- a. Any tree being relocated shall not be unnecessarily damaged during removal, transport or replanting of that tree.
- b. If the trees have a dormant period, they should be transplanted during that time. Trees should not be transplanted during periods of strong, dry winter winds or during droughts.
- c. Adequate space for root and crown development shall be provided.
- d. Trees shall be root and canopy pruned according to sound arboricultural standards prior to transplanting.
- e. During and following transplanting, the root ball and trunk shall be protected. The root ball must be kept moist at all times.



*"Tree Relocation"*



*"Palm Relocation"*



## public streetscape

## 1.3 Off-Street Parking and Loading

Minimum off-street parking and loading requirements shall conform to Article IV Off-Street Parking and Loading Requirements. Non-residential uses shall comply with the "Administrative Parking Standards Document".

The following criteria shall also be considered:

- a. Parking lots and other vehicular use areas are to be designed to be functional and aesthetically enhance neighborhood building, group of buildings, or facility they serve. A parking lot is to be considered a transitional space between the access (such as roads) and the building, group of buildings, or other outside spaces, including opportunities between two or more separate property owners that it serves. The parking lot, because it is viewed from surrounding buildings through a bird's eye view as well as at ground level, should be designed for aesthetic views of both.
- b. Parking and vehicular circulation between the street and the building, where encouraged in a specific district, should be limited where possible. Parallel parking between the street and the building is encouraged because it will result in better driveway definition and traffic calming. Shared parking and circulation is encouraged where ever practicable. Auto oriented and drive through uses, where permitted, should locate drive-through lanes away from street frontage. Automobile gasoline service stations should orient parking, car wash and service bays away from view of arterial streets.
- c. Off-street loading areas shall be located where they will not disturb adjacent uses and should not be the visual focal point of a driveway, parking area, adjacent properties, or the right of way. This may be accomplished by providing any or a combination of the following: masonry wall extensions of the building line, opaque landscape screening, berming, and through selective placement or orientation of the loading area.
- d. Developments which include outparcels shall be designed to provide safe and efficient vehicular and pedestrian circulation within the outparcel, between the out-parcel and the principle development and off-site. All pedestrian connections should be well marked and lighted.



*"Landscape enhances garage facade"*



*"Garage designed to screen views at street level"*



*"Parallel parking calms traffic"*

## public streetscape

- e. Sites requiring large areas of surface parking should attempt to distribute parking into smaller areas broken up by intervening areas of landscaping, open space and buildings where ever possible rather than aggregating parking into continuous street facing strips. On-site parking facilities should not be located along significant pedestrian corridors, greenways, or waterways. Whenever possible, parking facilities should be wrapped by functional building space, such as retails shops, office space, such that the parking is interior to the site rather than on the periphery.
- f. On-street parking should be employed whenever possible to encourage pedestrian activity and slow traffic.
- g. Parking areas must provide adequate drainage in accordance with city ordinance as well as requirements of Broward County's Environmental Protection Agency and the South Florida Water Management District as applicable to particular site.

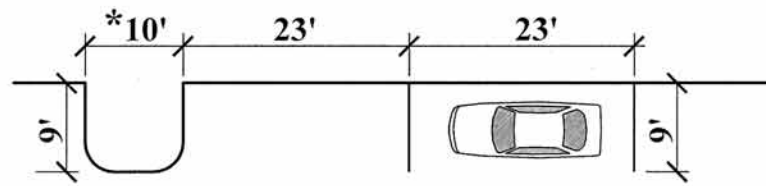


*"Open space within parking area"*



*"On-street parking protects pedestrians"*

## Parallel Parking

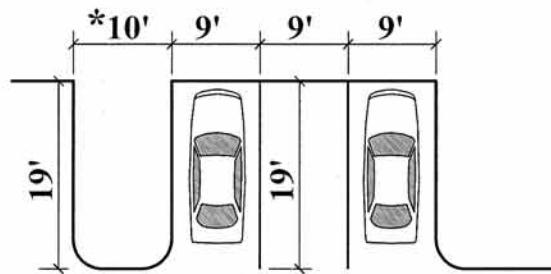


**Maneuvering Area**

13' One-Way

23' Two-Way

## 90 Degree Parking

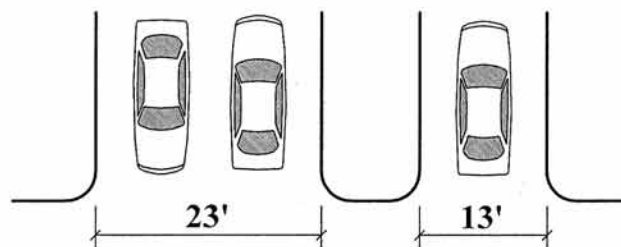


**Maneuvering Area**

23' One-Way

23' Two-Way

## Accessway



**Lot Entry**

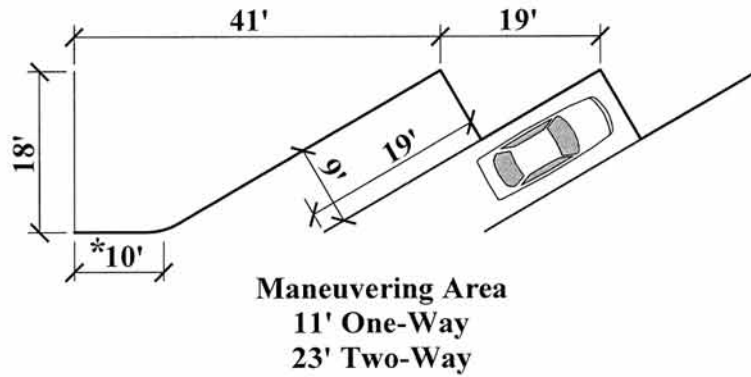
13' Min. One-Way

23' Min. Two-Way

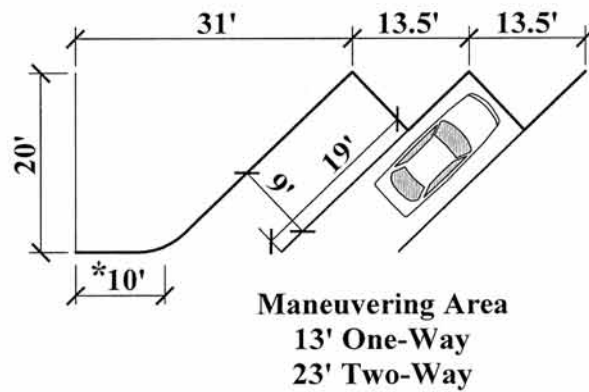
\* Required Landscape Buffer  
Refer to Section 32-384 (e) of the city's  
Land Development Code



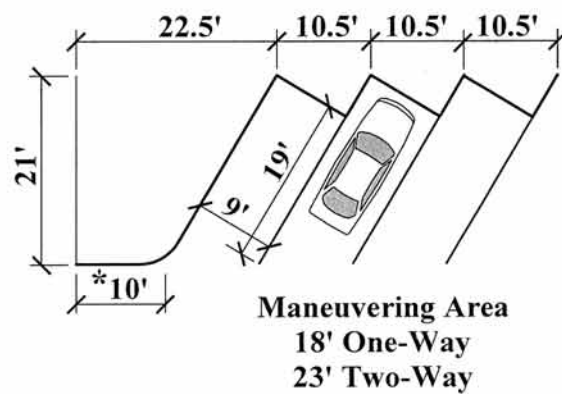
### 30 Degree Angle Parking



### 45 Degree Angle Parking



### 60 Degree Angle Parking



\* Required Landscape Buffer  
 Refer to Section 32-384 (e) of the city's  
 Land Development Code

## public streetscape

### 1.4 Dumpster Enclosures, Garbage / Mechanical Equipment Rooms

Mechanical equipment is necessary to the function of the buildings, which comprise a successful development. Unfortunately, space must be found for components that are sometimes large, noisy and unsightly. Mechanical equipment, particularly when added after the building is in use, can interrupt the streetscape and public views, decreasing the comfort and livability throughout the area.

Enclosures and Mechanical Rooms shall conform to the following criteria:

- a. When associated with a restaurant and/or drinking use, trash and garbage facilities are recommended to be within an enclosed, air-conditioned garbage room for new construction or when buildings are being substantially rehabilitated, if feasible.
- b. When located outside of the building, the facilities are preferred to be enclosed within a CBS opaque structure. The structure (including opaque gates) shall be painted to match the building, unless otherwise required by a special use district.
- c. Dumpster enclosures shall be designed in a manner as to visually screen the dumpster from adjacent view and shall be located in visually obscure areas of the site. The gates shall not be chain link, but must be wood, metal or other durable material per section 32-635 of the Hallandale Beach Code of Ordinances.
- d. Dumpster enclosures shall be placed in such a manner as to allow front end loader sanitation trucks to pick up garbage in a forward motion. Backing out of the sanitation truck is prohibited.



*"Building facade screens utility room"*



*"Heavily landscaped dumpster enclosure"*



*"Masonry dumpster enclosure"*

## public streetscape

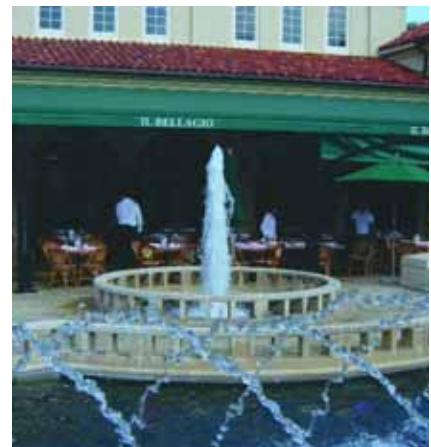
## 1.5 Awnings and Canopies

Pedestrian related concerns are a priority in the creation of a successful development. Overhead protection from rain and sun should be provided for pedestrians. Awnings have an impact on the appearance of the storefront and building and tend to bring pedestrians closer to shop windows and entrances. Consideration shall be given to the following where applicable:

- a. Buildings/storefronts should have awnings or other means to provide pedestrians with sun/rain protection unless physically unsuited.
- b. Continuous awnings over several stores are prohibited. Individual awnings should be distinct from its adjacent neighbor. When multiple awnings are attached to one building, awnings shall be of identical height and depth.
- c. Backlit awnings are prohibited. These awnings, because of their high visibility, become attention getting devices – such as a sign, rather than means to provide comfort and protection for the pedestrian. Such awnings overwhelm the appearance of the buildings they are attached to, detracting from architectural qualities. Awnings that incorporate subtle down-lighting in a manner which creates a discreet peripheral washing of the awning, may be appropriate in some instances. High gloss vinyl (plastic) awning, backlit and metal awnings are not permitted.
- d. Awnings shall be maintained in good repair, free from tears, fading or peeling. Awnings may be supported by poles and connected to the building underneath. Awnings needing vertical support columns are prohibited in the setback area.
- e. The awnings on corner buildings should continue around the corner for compatibility with building form and pedestrian patterns, wherever possible.
- f. Signs on awnings/canopies shall be consistent with Article IV of the sign code.
- g. Awnings should not be used where there is an existing projecting concrete sunscreen, except that a vertical awning valance may be suspended below the sunscreen with a clear height of 8 feet above the sidewalk.
- h. Awnings should utilize color schemes that blend with those of neighboring developments as well as consistency in color schemes for the site. Accent colors shall be chosen to enhance architectural details. Day-glo colors are prohibited. Solid color and broad striped fabric patterns are preferred.



*"Awnings have an impact on business entries"*



*"Awnings create a comfortable dining experience"*



*"Awnings utilize color scheme"*



## public streetscape

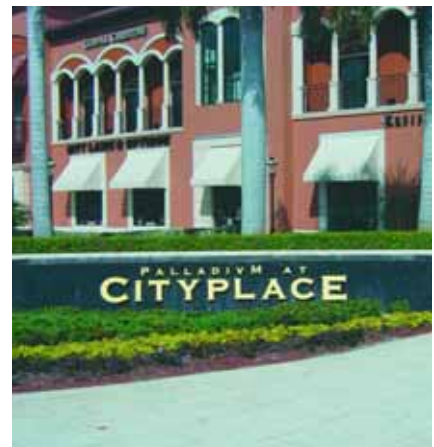
## 1.6 Signage

Signs should be carefully integrated with the site, landscape and architectural design content within which they are located. Size, shape and proportions should be compatible with the size and scale of the surroundings and should not compete with or obscure other design features of the site, landscape or structures.

- a. In residential areas and mixed-use developments that include residential uses, signage shall be located and illuminated in such a way as to avoid adverse impacts such as light or glare into residential units, yards or streets.
- b. Signage on buildings with multiple storefronts shall be of the same type and material, i.e.: channel letters illuminated, box sign, or pin letters, however, uniform color of such signage is not required to allow for diversity. Painted wood wall signs are prohibited in the corridors.
- c. Traffic control, directional and informational signage within a development shall be a specific decorative design, selected from samples, and must conform to the Broward County Manual of Uniform Traffic Control Devices.
- d. Primary materials of a monument sign and base should be of materials which provide an image of permanence such as stucco, cast stone, or metal. The sign cabinet and base should be of a high aesthetic quality and be at least four (4) inches wide.
- e. Bright colors on signage shall be limited to the sign message, trims and other accents of the sign. Garish electric or day-glo colors are prohibited.
- f. Internally illuminated awnings shall not be permitted.
- g. All signage is subject to the controls of the zone district in which it occurs and is subject to further review under the provisions of these rules and regulations.



*"Clearly illuminated entry sign"*

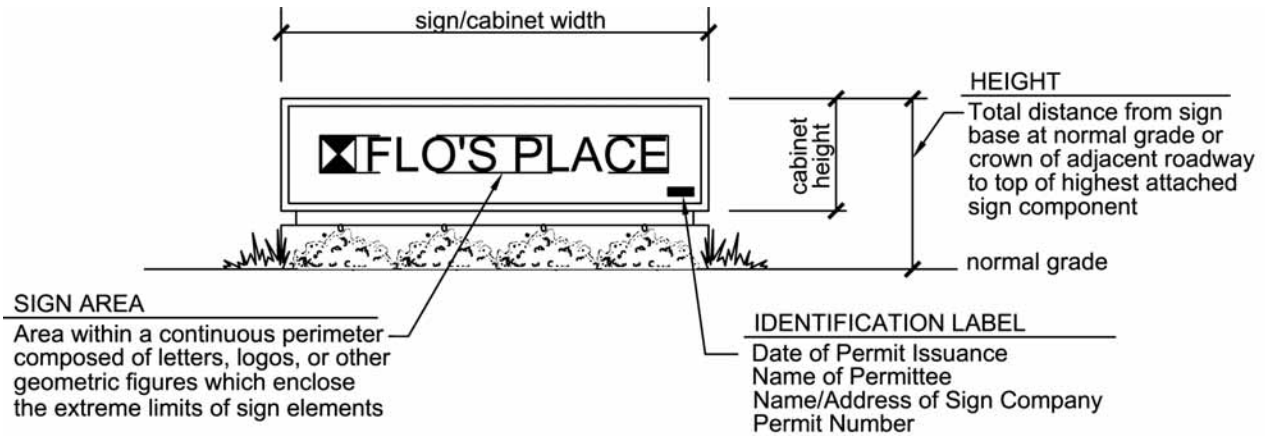


*"Identification signage clearly mark entry"*

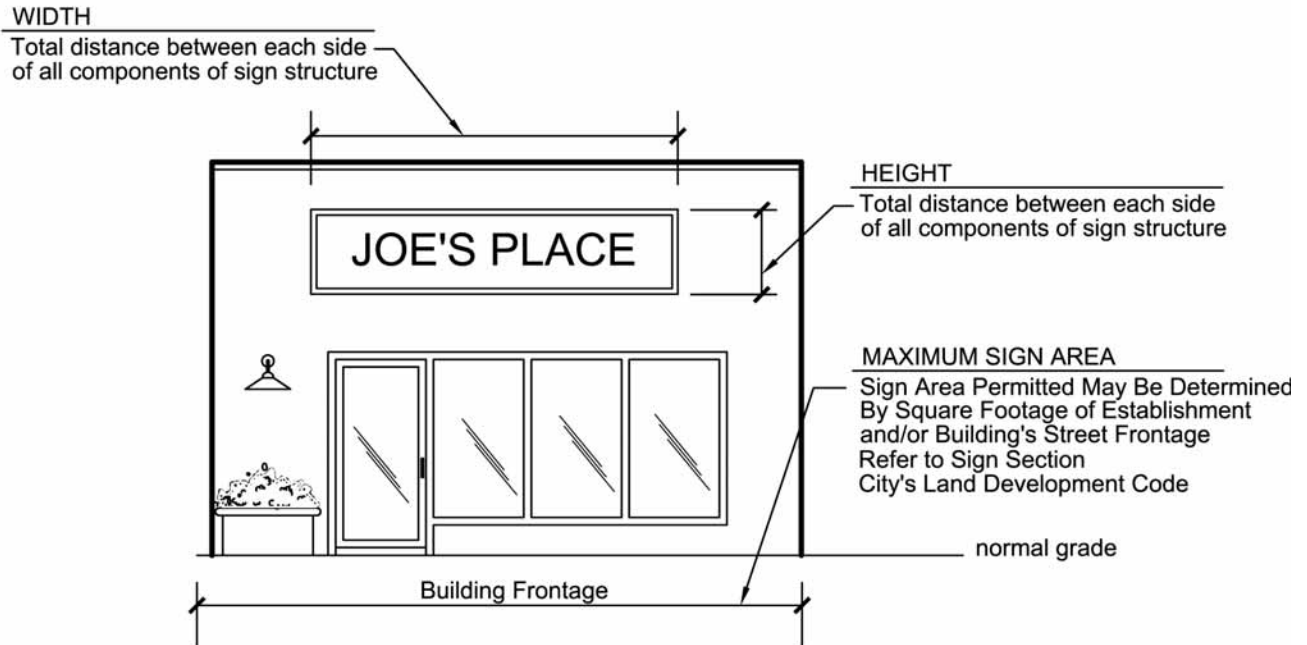


*"Directional signage reduces confusion"*

public streetscape



Ground Sign



Wall Sign

TYPICAL SIGN AREA

## public streetscape

## 1.7 Pedestrian Protection

The comfort of the pedestrian is crucial to developments. This is especially the case today, when people have come to expect the climate control found in contemporary buildings. Like much of the south, Hallandale Beach has many months of extremely hot and humid weather. The tendency to dash across a parking lot from an air-conditioned car into an air-conditioned building is normal behavior in Florida today, and so is the tendency to shop or eat only in places where parking can be found directly in front of the store or restaurant.

Making the transition to a pedestrian oriented streetscape will require special attention to the comfort of the walker. Because they are the economic base for development it is important that pedestrians are comfortable. They will choose the most comfortable route, avoiding blocks which are less comfortable or which create gaps in the continuity of the protection.

- a. Overhead cover at the sidewalk may provide continuous protection without being continuous itself, and must maintain a verticle clearance of 8'.
- b. Cover may take the form of either a projection from the building, an arcade, or a combination of the two. Arcades shall be open to the street, and can be outside of the public right-of-way.
- c. Projections may take the form of fabric awnings that are retractable, fabrics that are not retractable, or fixed non-fabric projected covers.
- d. Projected covers may be occupied by the building user, but should be accessed only from the building and not from the sidewalk. Where projected covers are occupied, they may also be supported by columns which fall in the right-of-way. Columns in the right-of-way should not interfere with pedestrians or emergency functions of the sidewalk.
- e. Where buildings have been permitted to pull significantly away from the property line, a free standing cover should be provided along the right-of-way. Owners are encouraged in this condition to provide pedestrian cover up to the property line or ingress/egress easement, but must have a 2' horizontal clearance from back of curb
- f. Existing buildings, which experience significant renovation, should provide pedestrian protection as well. Landmark buildings may comply by installing a detached cover in front of the building.



*"Covered corridor provides shade and rain protection"*



*"A clear path encourages strolling"*



*"Design elements create protection"*



## public streetscape

- g. Owners of existing buildings downtown are strongly encouraged to provide continuous pedestrian cover as per this guideline.

The physical nature of the streetscape should make people walking there as safe as possible, and should make them feel a sense of safety as well. It may be impractical to assume that effective barriers could be provided along curbs of every street downtown, protecting pedestrians from the possibility of being struck by a car, but a degree of protection can be created in fairly easy and inexpensive ways.

Perhaps the best protection for the sidewalk would be cars parked along the curb. Parking meters would provide some protection too. Where there is no parallel parking at the curb, small bollards, heavy planters or other similar devices may provide some protection. Parking along the street edge can provide a buffer between busy automobile traffic and pedestrian movement. It also acts as a traffic-calming feature, slowing drivers in the curbside lane.

- a. Barriers from cars should be provided along the sidewalk edge, in the form of cars parked in legitimate parking spaces, trees, planters, or bollards.
- b. Curbside parking is encouraged along all busy downtown streets especially where right-of-ways measure less than eighty (80) feet in width.
- c. Parallel street parking must provide curbing and street drainage. Landscaped planter islands to be a minimum of 8' by 8', with irrigation and up lighting. The spacing of the planter islands to be a maximum of 5 parallel parking spaces or the beginning and end of the property, which ever is less.

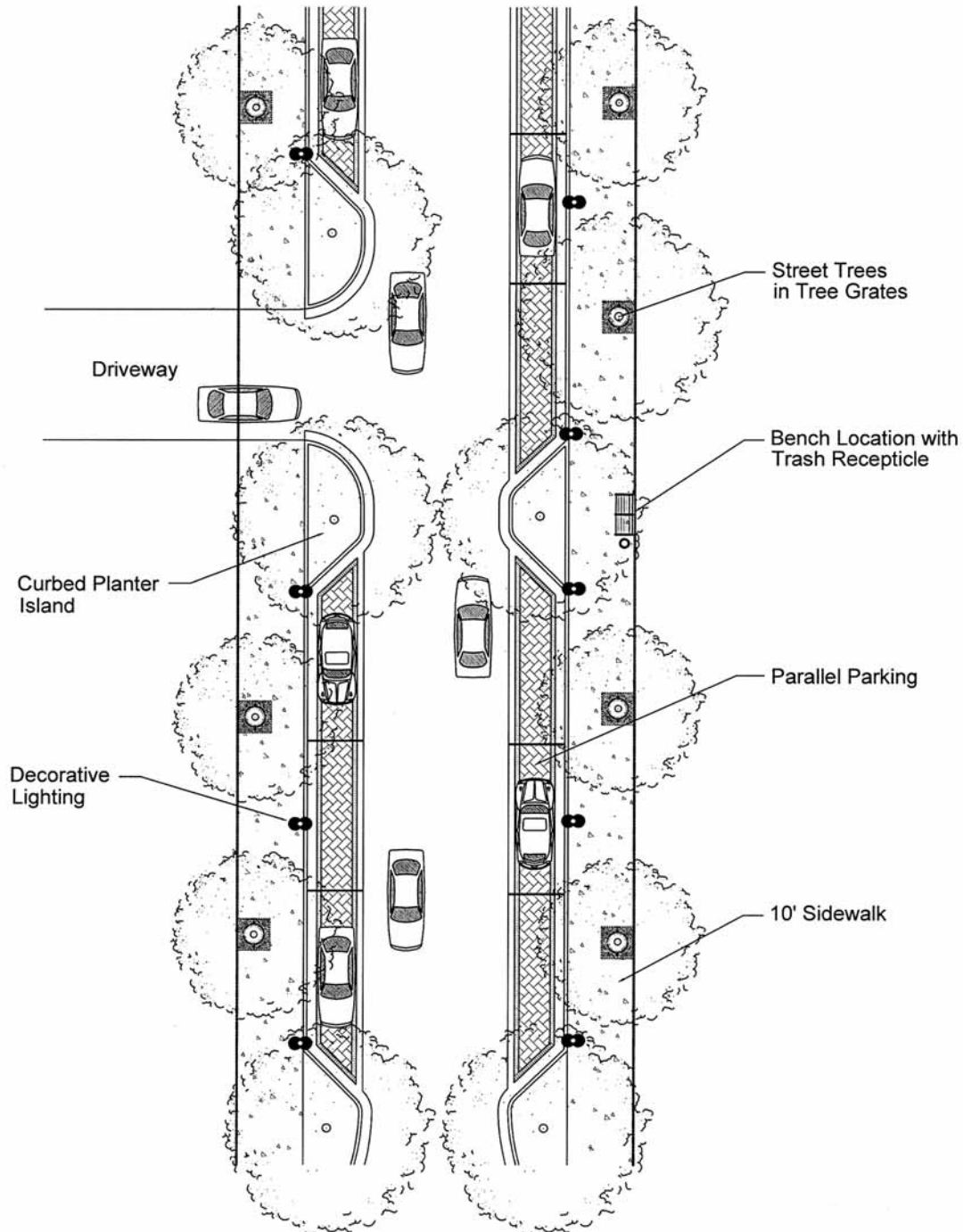


*"Landscape offers a barrier between pedestrians and automobiles"*



*"Parallel parking creates protected sidewalks"*

public streetscape



Streetscape Alternative

## public streetscape

## 1.8 Minimize Curb Cuts

**Commercial Developments:**

The safety and comfort of people walking on a sidewalk is of greater concern than the convenience of a driver; pedestrians are the primary concern. Every time a car crosses the sidewalk there is a potential danger and inconvenience to the pedestrian. For this reason, places where cars cross the sidewalk should be minimized throughout the development and if concentrated at mid-block. Contiguous developments shall coordinate circulation plans to minimize curb cuts including sharing of centralized driveways through joint access agreements, and through cross-access agreements.

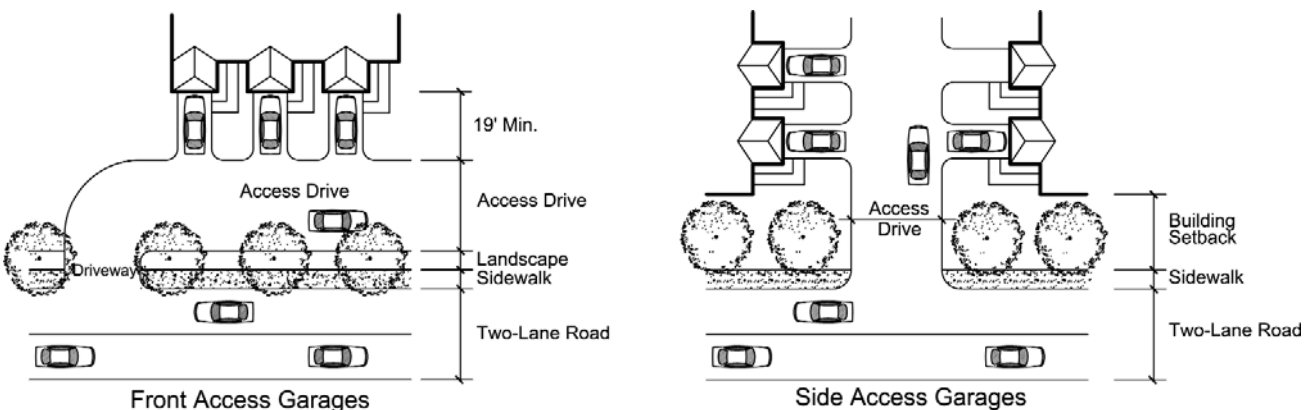
**Single-Family Developments:**

Driveways along residential streets should be no closer than fifty (50) feet, except where two driveways may be located adjacent or nearly adjacent to each other, or where lots are narrower than fifty (50) feet. Where lots are narrower than fifty (50) feet, the maximum separation should be provided, given the lot's width, with a minimum separation of thirty-two (32) feet.

**Multi-Family Developments:**

Driveways to garages within multi-family developments shall not have parking spaces which back into the right-of-way. The use of separate lanes or access drives which intersect the public right-of-way which serves a cluster of units is required.

Properties fronting state or county rights of way must obtain driveway connection permits from F.D.O.T. or Broward County respectively. Driveways must maintain minimum offsets from property lines as specified by city ordinance.



Acceptable Multi-Family  
Garage Access Options



## public streetscape

## 1.9 Streetscape Environment

Streetscape amenities such as benches, trash receptacles, planters, pole lights, kiosks, telephones, news stands, drinking fountains and bike racks enliven and support the public domain. Café tables in the right-of-way can bring activity to the street.

Furnishings need to be attractive, pedestrian-friendly, durable and in harmony with the overall theme for which they are provided. Although street furnishings should be constructed of low maintenance materials, they should be implemented in conjunction with a maintenance strategy. The frequency and type of attention allocated to these elements should be a critical factor in their selection. Street furniture may be located within a development or in the right-of-way or public use easement, such as bus shelters and news racks.

The following criteria shall be considered:

- a. Street furniture may include benches, trash receptacles, telephones, water fountains, and clocks where appropriate. Street furniture may be fixed to the sidewalk if adequate clear passage for pedestrians and emergency access is provided.
- b. Café tables and kiosks may occupy a portion of the public right-of-way if adequate clear passage for pedestrians and emergency access is provided, as specified in the American Disability Act (ADA).
- c. The design of street furnishings should unify areas with distinct character. Participation from private property owners is encouraged.
- d. Appropriate plantings may be provided as well.
- e. News racks within the district shall be uniform in color and adhere to the requirements of the News Rack Ordinance #1998-29. Only modular news racks are permitted within the city rights-of-way or private property, except that, where only one individual rack is located in the area, the Director may allow a single news rack if deemed appropriate. All modular and single news racks in the public right-of-way shall be painted black. News racks on private property visible from public view shall be painted black.



*"Benches provide seating and harmony with overall theme"*



*"Planters and lights add distinctive character"*

### public streetscape

- f. Bus shelters within the city shall be uniform in material, style and color as established by the city.
- g. Trash receptacles should be placed at points of use – next to benches, shelters, outside restaurants and adjacent to residential entrances and in quantities that properly serve those uses. Numerous, smaller units are preferable to a few larger bins.
- h. Bicycle racks encourage bicycle traffic in an urban environment. Bicycling is an activity which brings families and groups of people to pedestrian friendly districts. They should differ in appearance based on their location and the need for bicycle parking in specific areas. Bicycle racks should be provided in a development whenever feasible.
- i. All streetscape furniture and placement shall meet CEPTED and ADA requirements.



*"Uniform style of bus shelters"*



*"Trash receptacles placed at point of use"*



*"Bike racks should be provided when feasible"*

## public streetscape

## 1.10 Lighting

Lighting comes in two types: functional and ambient. Lighting in addition to Florida Power and Light Standards shall be designed from the standpoint of safety and reviewed as an integral part of the overall development.

Lighting shall conform to the following criteria

- a. Parking lot fixtures are to be selected not only for their functional value, but also for their aesthetic qualities. They are to be considered furniture of the parking lot visible both day and night. Light fixtures used in the district shall be decorative for new development or redevelopment within public view and are encouraged throughout the development. The decorative fixtures shall be of a style that compliments the development. Cobra heads are prohibited within a development. Shoe box units may be used but are discouraged at entrances and exits.
- b. Parking area lighting should compliment the lighting of adjacent streets and properties and should use consistent fixtures, source colors and illumination levels.
- c. Poles should be placed to provide a unified, organized appearance throughout the parking area or development and should provide even and uniform light distribution. The use of a greater number of low fixtures in a well-organized pattern is preferred over the use of a minimum number of tall fixtures.
- d. Outdoor storage areas including auto and truck parking and storage should be illuminated from poles similar to those used for parking lot lighting, but at lower illumination levels.
- e. Parking lot and security lighting shall be designed to direct light into the property.
- f. Security lighting should be limited to low-intensity specialty fixtures. The light source should not be visible from the street or adjoining properties. Other wall mounted security lighting is discouraged.
- g. Building lighting should be used to highlight specific architectural features. Lighting of architectural features should be designed with the intent of providing accent and interest or to help identify entry and not to exhibit or advertise buildings or their lots.



*"Street lighting with banners"*



*"Lighting highlights architectural features"*



### public streetscape

- h. When pedestrian lighting is used in conjunction with street lighting, the pedestrian lighting should be clearly distinguishable from the ambient street lighting to clearly define the pedestrian path of travel.
- i. When adjacent to pedestrian circulation and gathering areas, parking area lighting should not overpower the quality of pedestrian area lighting.
- j. Lighting should be designed to provide even and uniform light distribution without hot spots, dark spots or glare. Lighting should be designed to minimize dark areas that could pose a security concern near pedestrian areas. Pedestrian circulation systems should be highlighted by visible light sources that clearly indicate the path of travel ahead.
- k. Placement of fixtures should provide a coordinated and organized appearance that facilitates uniform light levels and works with the placement of sidewalks, landscaping, signage, building entries and other features to contribute to the overall continuity of the streetscape and development.
- l. Accent lighting of landscape areas should be low level and background in appearance.
- m. The color of the light sources shall be consistent throughout the project. High pressure sodium lamps are not permitted.
- n. Decorative accent lighting of landscape features, at entrances and exits is recommended.



*"Pedestrian lighting should clearly define path of travel"*

## public streetscape

### 1.11 Access, Public Rights-of-Way and Utilities

Off-site improvements associated with new development or redevelopment as may be required by Article IV, Development Review Requirements, shall be subject to the following:

- a. Public use of existing and future rights-of-way should be enhanced through thoughtful vehicular and pedestrian improvements, and good maintenance. Public rights-of-way no longer needed for their original intended use should be re-evaluated with an eye toward augmenting greenways, blueways, parks or school space. Existing public rights-of-way should be used directly to benefit the public.
- b. Off-site improvements shall be consistent with the City's established specifications for such improvements or be subject to City Commission approval.
- c. Drainage and disposition of natural and storm water shall be designed to be compatible with existing systems.
- d. New, replacement, or upgraded on-site utilities and other services shall be provided underground. If at all possible, new, replacement or upgraded utilities and other services provided within public rights-of-ways must also be placed underground. Where an approved master plan of improvements calls for underground utilities and other services in the right-of-way, the improvements should be consistent with and implement the master plan.

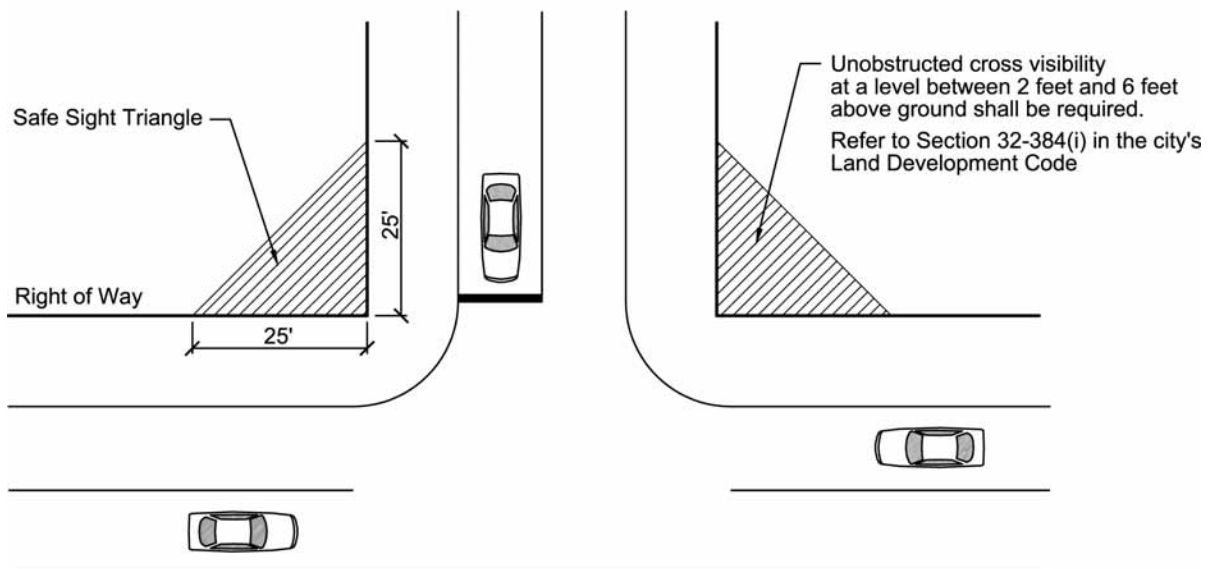
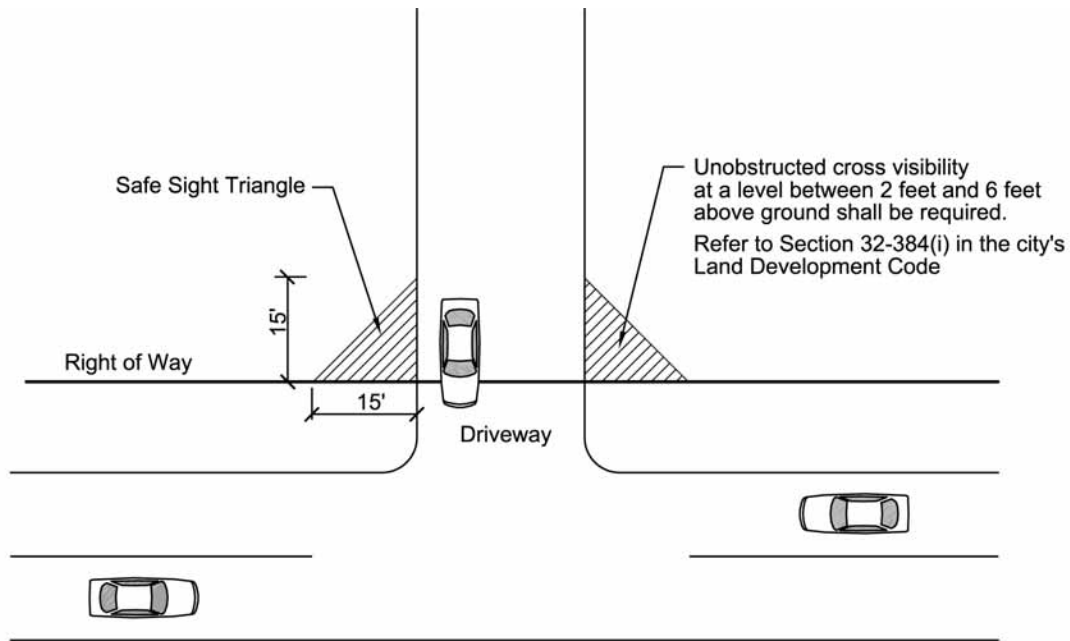


*"Public oriented spaces enhance right-of-ways"*



*"Artistic features in public spaces"*

public streetscape



# Vision Clearance Intersection Triangle



## public streetscape

## 1.12 Outdoor Dining and Sidewalk Cafes

Café tables in the right-of-way can bring activity to the street. They can provide a wonderful means of people watching for diners and pedestrians. Consideration should be given to unification of these elements within a block from street to street.

Restaurants and bars are also encouraged to provide outdoor service in courtyards or arcades. Sidewalk cafes on the public right-of-way may be allowed subject to the approval of the City Commission. Outdoor restaurants, bars or sidewalk cafes must be associated with an adjacent licensed restaurant and comply with all other zoning regulations and conform to the following criteria:

- a. Placement of tables, chairs and related equipment shall be situated to insure that a minimum of five (5) feet straight pathway on the sidewalk is maintained at all times as an unobstructed pedestrian path.
- b. Serving through windows is not permitted.
- c. Food preparation shall only occur in the enclosed restaurant.
- d. Because tables form sufficient advertisement, no additional signs for the sidewalk café are permitted.
- e. Outdoor furniture shall be substantial enough not to blow over with normal winds.
- f. All outdoor furniture and fixtures shall be tastefully compatible and approved by the zoning department.
- g. All disposable table materials such as plates, glasses, and napkins shall be imprinted (stickers may be used) with the name of the café. This regulation is to control litter.
- h. Sidewalk cafes shall receive a Revocable Permit subject to the procedures established by the City.
- i. Please refer to Sections 32-571 through 32-600 of the Zoning Code for applicable rules and regulations regarding outdoor restaurants and bars.



*"Cafes become gathering spots"*



*"Outdoor seating enhances cafe identity"*



*"Smaller seating areas create a personal setting"*

public streetscape

### 1.13 Crime Prevention Through Environmental Design (CPTED)

The U.S. Government "Crime Prevention Through Environmental Design Program" (CPTED) incorporates architectural solutions to reduce the opportunity of crime. Elimination of recessed entryways, provision of adequate lighting and proper design of spaces will reduce the possibility of criminal activity.

- a. Building mounted lighting should be installed on alley frontage and side yards. This is particularly recommended at service/delivery entrances.
- b. Windows in the alleys or sides provide the appearance of natural surveillance and may discourage break-ins. Such windows should not be blocked up.
- c. See through fences and gates of metal pickets should be located to discourage uncontrolled access to service/delivery areas.
- d. Hiding places and blind corners should be eliminated from site/building, where possible.
- e. The concept of natural surveillance, visibility by the public (shoppers, pedestrians, motorists, and/or personnel) should be incorporated into the design where possible.
- f. Landscaping should be designed to discourage crime. Tree heights/spread should allow sufficient visibility, not completely block views of/from doors and windows. Shrubs should not be planted where they may become hiding places.
- g. Fences should be largely transparent. Low fences/walls are preferred.



*"Hiding places on-site should be eliminated"*



*"Site features maximize visibility"*

planned development district

**Section Two**

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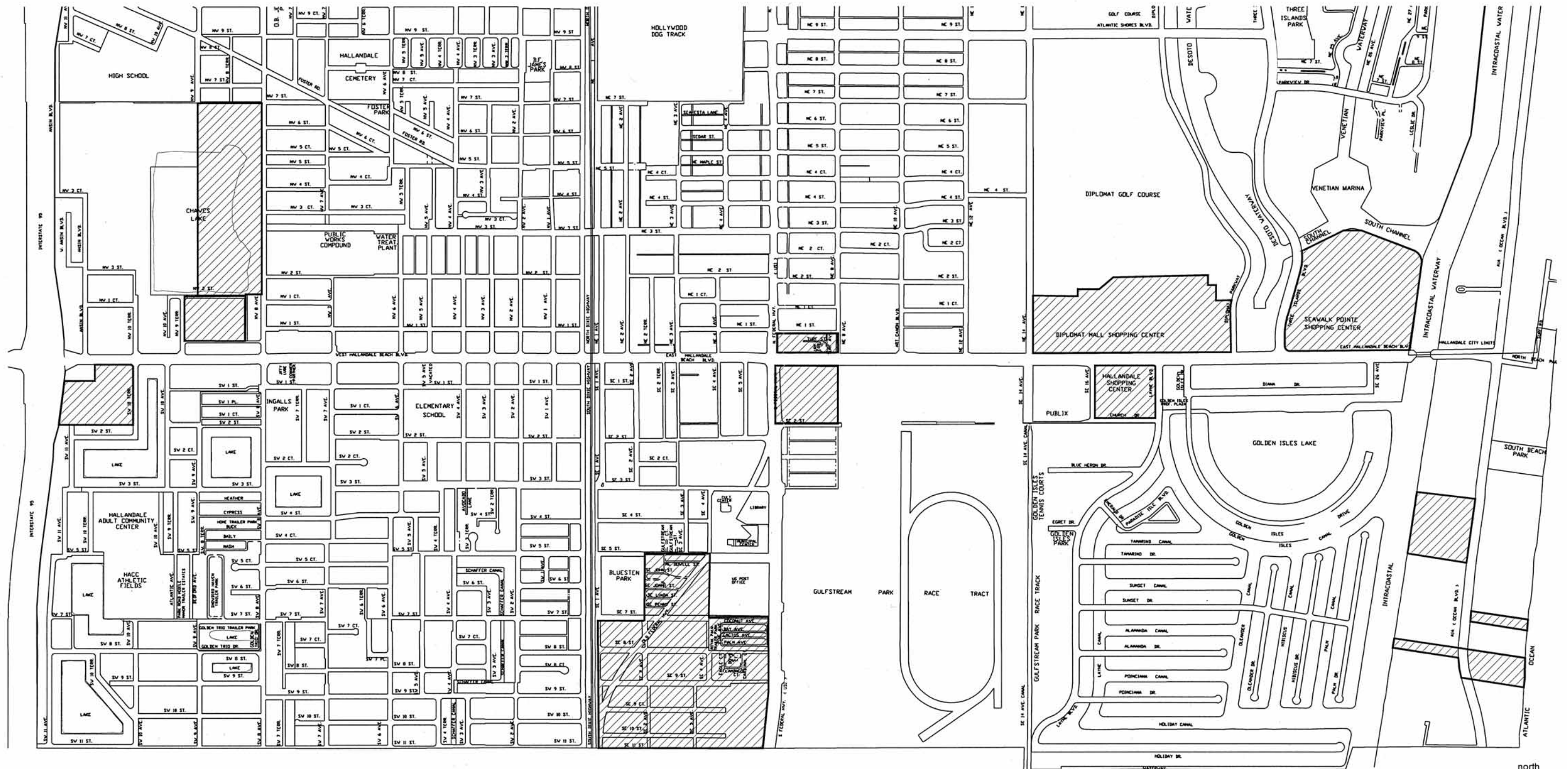


## planned development district

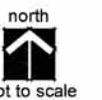
### 2.0 Intention Statement

The Planned Development District is intended to create flexibility for the developer and the city officials by establishing a vehicle for rezoning which allows for creativity in the design process and public space enhancement. It also permits greater land use intensity and density to encourage higher quality and more innovative development. The transition from adjacent properties and public spaces to the planned development property is a major consideration for all.

## 2.1 planned development district map



 planned development district



## planned development district

### 2.2 Administrative Process

A Planned Development District (PDD) rezoning application shall be processed in accordance with the Zoning and Land Development Code Section 32-174 PDD Planned Development District and Design Regulations.

- a. A Development Agreement is a legislative action by the City Commission and is submitted in concert with the application for Major Development Plan as a component of the planned development process.
- b. Prior to submitting an application for a PDD rezoning request, an applicant shall confer with City staff at a pre-application conference. At this conference, the applicant can obtain information and guidance before entering in to the application and negotiation process and incurring substantial expense in the preparation of data relevant to the preparation of the concept plan and major development plan.
- c. Upon submission of a major development plan application, the applicant will negotiate the development details of a development agreement with City staff for further consideration by the City Commission.
  - (1) Upon the completion of "c" above, the City Manager will complete a development agreement for consideration by the City Commission during the major development plan review process.
  - (2) In no case shall a major development plan application proceed forward until the aforesaid development agreement is prepared.
- d. A development agreement shall include at a minimum the following:
  - (1) A legal description of the property subject to the agreement and the names of its legal and equitable owners.
  - (2) Development details pertaining to the property including population densities and building intensities; height, bulk, and yard requirements; open space; traffic and roadways; utilities and services including water and sewer, street lighting, garbage, storm drainage and public safety protection; landscaping; and off-street parking and loading areas.
  - (3) A description of any public and private reservations, dedications and conveyances.
  - (4) A finding that the development permitted of proposed is consistent with the City's Comprehensive Plan and land development regulations.
  - (5) A description and schedule for the phasing and construction of the development.
  - (6) A description of all City development permits required or to be approved for development of the property.
  - (7) A description and statements pertaining to amendments and changes to the agreement; breach of agreement; destruction of the property; controlling documents and binding effect of the agreement; effective dates; and conflict provisions.



## planned development district

- (8) Listing of exhibits and controlling documents.
- (9) A description of any incentives offered by the City and accepted by the applicant.
- (10) Provisions for periodic review if necessary.
- e. At the request of the applicant and upon the recommendation of staff, the City Manager may waive any required component of the development agreement.
- f. The standards contained in the Zoning and Land Development Code and the Hallandale Beach Code of Ordinances are the minimum development standards for the purpose of negotiating a development agreement.
- g. All codes and regulatory provisions of other regulatory agencies are considered minimums and shall be conformed to unless relief is granted by the respective agency.
- h. The Development Agreement to be designated (Exhibit A) and the implementing Major Development Plan to be designated (Exhibit B) shall be considered the controlling documents/exhibits for the PDD site. Further development and implementation of the PDD may occur via additional major development plan approvals if necessary.
- i. Amendments to Planned Development Districts (PDD) controlling documents will be accomplished as detailed within the Development Agreement for the specified PDD.
- j. In the event that the major development plan (Exhibit B) and the development agreement (Exhibit A) are found to be in conflict, the applicable provision of the development agreement shall prevail.
- k. The PDD development shall be undertaken and carried out in accordance with the Hallandale Code of Ordinances in effect on the effective date of the development agreement except for those exceptions and variations as set forth in the negotiated development agreement or major development plan exhibits attached thereto.
- l. There shall be strict adherence to the approved development agreement and major development plan. Any substantive change or amendment to a PDD shall be addressed in accordance with Section 32-174 and can only be made by written agreement executed by the parties thereto and approved by the City Commission unless specified otherwise within the Planned Development District particular development agreement.
- m. In the event all or a portion of a PDD should be destroyed by storm, fire or other disaster, the developer, successors or assignees and/or the association shall have the right to rebuild and/or repair so long as there is strict compliance with the major development plans record.
- n. In the event that the developer, in the opinion of the City Commission has materially breached the PDD development agreement, the developer shall cure the breach within thirty (30) days. In the event the developer is unable or unwilling to cure the breach and abide by the development agreement, the City shall exercise its right to take appropriate legal action for the purpose of curing the breach.
- o. The PDD development agreement shall be binding upon the developer, its assignees and successors in interest, and the City. In the event that any portion or section of the development agreement is determined to be invalid, illegal, or unconstitutional by a court of competent jurisdiction, such decision shall in no manner affect the remaining portions or sections of the specific development agreement.

## planned development district

- p. Incentives: As a part of the negotiated development agreement, the City may provide various incentives to attract development, redevelopment or improve the quality of development design subject to the approval of the City Commission. The following are examples of the incentives which may be negotiated:
- (1) waiver or reduction of demolition fees;
  - (2) waiver or reduction of development review fees;
  - (3) in eligible areas, grants or low interest loans may be provided for façade improvements, installation of drainage improvements, utility installations and relocations, regrading and sodding of swales, landscape and streetscape improvements;
  - (4) for CDBG eligible projects, the City can enter into a private/public partnership whereby the City may, based on the availability of CDBG and or CRA funds, purchase the property and or building, providing a favorable lease or purchase rate, pay for design services and/or site preparation, cover construction equipment, personnel, improvement and rehabilitation and/or maintenance.

## planned development district

## 2.3 Dimension and Design Regulations

All site development standards and related development standards of the Zoning and Land Development Code are to be negotiated between the City and the applicant, as part of the development process in accordance with the Planned Development District regulations and all other applicable City Codes, regulations, and standards set forth in this manual.

The site development standards, landscaping and off-street parking requirements of the Zoning and Land Development Code shall apply to PDD master development plan proposals except as otherwise identified herein. The city staff with advice and consent of the City Commission may further modify these development standards if they are determined to significantly inhibit area or structural improvement efforts. Such modifications may be approved concurrently with the major development plan process and without variance or other form of modification applications. The City Commission may attach conditions to such modifications in order to compensate for the impact of modified development standard (s) on the project and surrounding areas.

a. Landscape treatment/improvements: Generally, landscaping shall conform to the applicable landscape requirements found in Article IV Landscaping Requirements. However, in no case shall landscaped areas be less than the stated requirements by regulatory agencies regarding stormwater drainage controls. These guidelines shall be utilized as a starting point and may be modified in the development process if the applicant's master development plan exhibits a more suitable plan based upon good planting and development practice. In addition the following provisions shall be considered:

- (1) The site should be planned to accomplish a smooth transition from adjacent properties that differ in land use and/or design philosophy by the use of site breaks such as screens, berms, fences, and walls for aesthetic purposes and all fences and walls shall contain design elements which blend in with the overall architectural design theme of the development proposal.
- (2) The installation of layered building foundation landscaping is required.



*"Example of a well-planted swale"*



*"Landscape treatment acts as transition feature"*



## planned development district

- (3) Employ landscape elements that define walkways and circulation patterns with consistent plant material patterns, color at intersections, reinforced site lines and focal points.
  - (4) Selection of plant materials with consideration for building design, growth patterns, colors, texture, spacing, shade, and compatibility with site activity. Employ landscape elements of a variety of heights and multi-level plantings to achieve finished mature landscape defines entries, building massing, and hierarchy of circulation patterns.
  - (5) Incorporation of buffers and berms for grade diversity, separation, and visual control.
  - (6) Off-site canopy street trees of a twenty (20) foot minimum height at the time of planting and of the same or complimentary species shall be provided every twenty-five (25) linear feet. Royal palms shall be the preferred street tree for those developments on Hallandale Beach Boulevard and Federal Highway (U.S. 1), and A-1-A.
- b. Dumpster enclosure: Dumpster enclosures shall be of masonry construction, finished in accordance with the same standards indicated in Section a. (1) above. Dumpster enclosures shall be located and designed in such a manner as to visually screen the dumpster from adjacent or private properties, or rights-of-way as determined by staff and shall be locked in visually obscure areas of the site. The walls of the dumpster shall be the same building materials as the structures.
  - c. Off-street parking and loading: City staff has the flexibility to require more restrictive requirements beyond Article IV of the Off-Street Parking and Loading Requirements.



*"Landscape elements define circulation patterns"*



*"Design to allow for visual control"*



*"Conforming parking design"*

## planned development district

- (1) Off-street loading areas shall be located where they will not disturb adjacent uses and should not be the visual focal point of a driveway, parking area, adjacent property, or right-of-way. This may be accomplished by providing any or a combination of the following: masonry wall extensions of the building line, opaque landscape screening, berming, and through selective placement or orientation of the loading area,
- d. Pedestrian-vehicle conflicts: A Planned Development shall be designed to minimize pedestrian vehicle conflicts. The circulation system shall discourage the inefficient or excessive use of the automobile. Walkways and circulation patterns should be defined through variations of pavement, alternate paving materials, texture and sign placement. Pedestrian crosswalks should be incorporated through landscape areas to avoid impervious surface masses.
  - e. Building and site design relationships. Buildings should demonstrate compatibility in materials and consistency in style throughout all exterior elements by the following:
    - (1) Being compatible with adjacent land uses in terms of scale and lot coverage;
    - (2) Using colors and building materials that are consistent with an approved existing scheme. As a part of the approval process all new development or redevelopment will be subject to a color and materials review.
    - (3) Using building wall extensions to connect structures and other site elements;
    - (4) Incorporating canopies or awnings to enhance building character and shelter pedestrians from the elements; and
    - (5) Or as proposed by the applicant using good planting design and development practices.



*"Masonry walls and Landscape screen loading areas"*



*"Consistant colors and building materials"*

## planned development district

- f. Signage: Signage shall be considered within the overall context of the site and the surrounding community. The selection of materials and the type of construction should not be based solely on cost or availability. In addition:

- (1) All signs on site shall complement the architectural style and scale of the buildings they relate to and should be designed as an integral part of the overall site development plan. Each sign shall be compatible with signs on adjoining premises and shall not compete for attention.
- (2) Sign color, materials and lighting should be chosen based upon the compatibility with exterior colors of the building.
- (3) Lettering styles and graphic symbols should be held to the minimum needed to convey the nature of the business.
- (4) Consideration should be given to the incorporation of position, means of attachment, materials, and lighting into lease agreements so that future tenants are held to the approved standards.

- g. Utilities and other mechanical equipment: All utility lines should be located underground. Mechanical equipment or other utility hardware on the ground should be located away from high use areas and main entrances. Equipment shall be screened with a fence, plant materials, or housed in a structure compatible with surrounding structures.

Solar energy collection devices should be mounted in a manner that preserves the property's character defining features. Devices not attached to a building should be located only in those areas obscured from public view. Exposed hardware, frames and pipings shall be finished to be non-reflective and consistent with the approved color scheme of the building.

Mechanical equipment attached to the side of a building shall be camouflaged in such a manner as to be hidden from view, preferably equipment shall be "stealth" in design.

Mechanical equipment on a roof of a building shall be kept as low as possible and painted to blend with the background and be screened from view of adjoining properties.



*"Signage complements architectural style"*



*"Sign color and materials compatible with building exteriors"*



## planned development district

- h. Open space relationships: Landscaped and usable open spaces shall be designed as integral parts of the site and building design. The location and configuration of usable open space should be so designed as to encourage social interaction, maximize its utility and facilitate maintenance. Provision of public spaces with desired features including fountains, decorative benches, and lighting, etc., should be included as favorable elements within a development.
- i. Access requirements: All major development plans shall provide shared access easements to adjoining properties and projects where the adjoining owner agrees to minimize the number of curb cuts and driveways onto adjacent public streets and highways.
- j. Off-site improvements/medians: If as a part of the conceptual plan review process, off-site improvements are required, they shall be consistent with City standards or subject to City Commission approval.
- k. Outparcel development: Development of an outparcel created prior to a PDD (Planned Development District) rezoning and held under separate ownership or in the event that an outparcel is sold after approved within the context of the overall development plan or scheme and these guidelines.

Consideration shall be given to the following:

- (1) Provision for drainage and disposition of natural and storm water shall be designed to be compatible with existing systems.
- (2) Proposed utility services shall be designed to be compatible with existing services, however this shall not preclude the installation of services underground for new development.
- (3) Proposed layout of structure, site elements and use shall further the approved conceptual layout and intent of the overall site development plan.
- (4) Vehicular and pedestrian circulation shall be designed to provide safe and efficient circulation within the outparcel, between the outparcel and the principal development and off-site.



*"Landscaped and usable spaces as integral part of design"*



*"Benches, fountains and planters are favorable elements"*



*"Planted median within the development"*



## planned development district

- (5) Proposed fences, walls, screens, planting and hedges shall be designed to be compatible with the overall major development plan.
- (6) Evaluation of impacts shall be based on the cumulative impacts of principal site and all outparcel development.
- (7) Proposed signage shall be consistent with approved overall development signage plan.
- (8) Utility areas and uses, including dumpsters, loading zones, outside storage areas, mechanical equipment and utilities shall be located in such a manner as to not create a visual focal point from adjoining property or right-of-way. Waste receptacles shall be screened by brick or masonry walls. Outside storage areas shall be partially enclosed to restrict views from adjacent parcels. Mechanical equipment shall be screened with plant material or housed in a structure that is in harmony with surrounding structures.
- (9) Landscaping types, arrangement and species of proposed trees and shrubs should be compatible with the landscaping on the adjoining site, unifying the developments.
- (10) A gradual transition between outparcel and principal development can be achieved through setbacks, building height, walls, landscaping, building design and materials. Outparcel development shall be designed as to give the appearance of overall unity of design.

The development of outparcels under the same ownership as the principal development shall be subject to these design guidelines (one through ten above).

- I. Street furnishings are a very important element to a successful streetscape environment. Furnishings shall be required when the development includes public gathering spaces.



*"Signage shall be consistent with overall signage plan"*



*"Mechanical equipment effectively screened with Landscape"*



*"Landscape is integral in screening development from right-of-way"*

## planned development district

- m. Lighting: All lighting, in addition to Florida Power and Light Standards, shall be of a design and size compatible with, and reviewed as an integral part of the overall development design. For proposed development located in any of the City's special planning districts, lighting may also be reviewed for compatibility with district wide goals, objectives, and requirements of Crime Prevention Through Environmental Design (CPTED) and should meet the requirements of the Zoning Code.
- (1) Light fixtures used in the District shall be decorative for new development within public view and are encouraged throughout their development. The decorative fixtures shall be of a style that compliments the development. Cobra heads are prohibited within a development. Shoebox units may be used but are discouraged at entrances and exits.
  - (2) Ornamental street lamps should be installed, where feasible in a style similar to the fixtures already existing in the District.
  - (3) The use of mounted building lights is discouraged, but if used, the light source should not be visible off site.
  - (4) The color of light sources shall be consistent throughout the project. High-pressure sodium lamps are not permitted.



*"Decorative street lights complement the development"*

## planned redevelopment district

### Section Three

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## planned redevelopment overlay district

### 3.0 Intention Statement

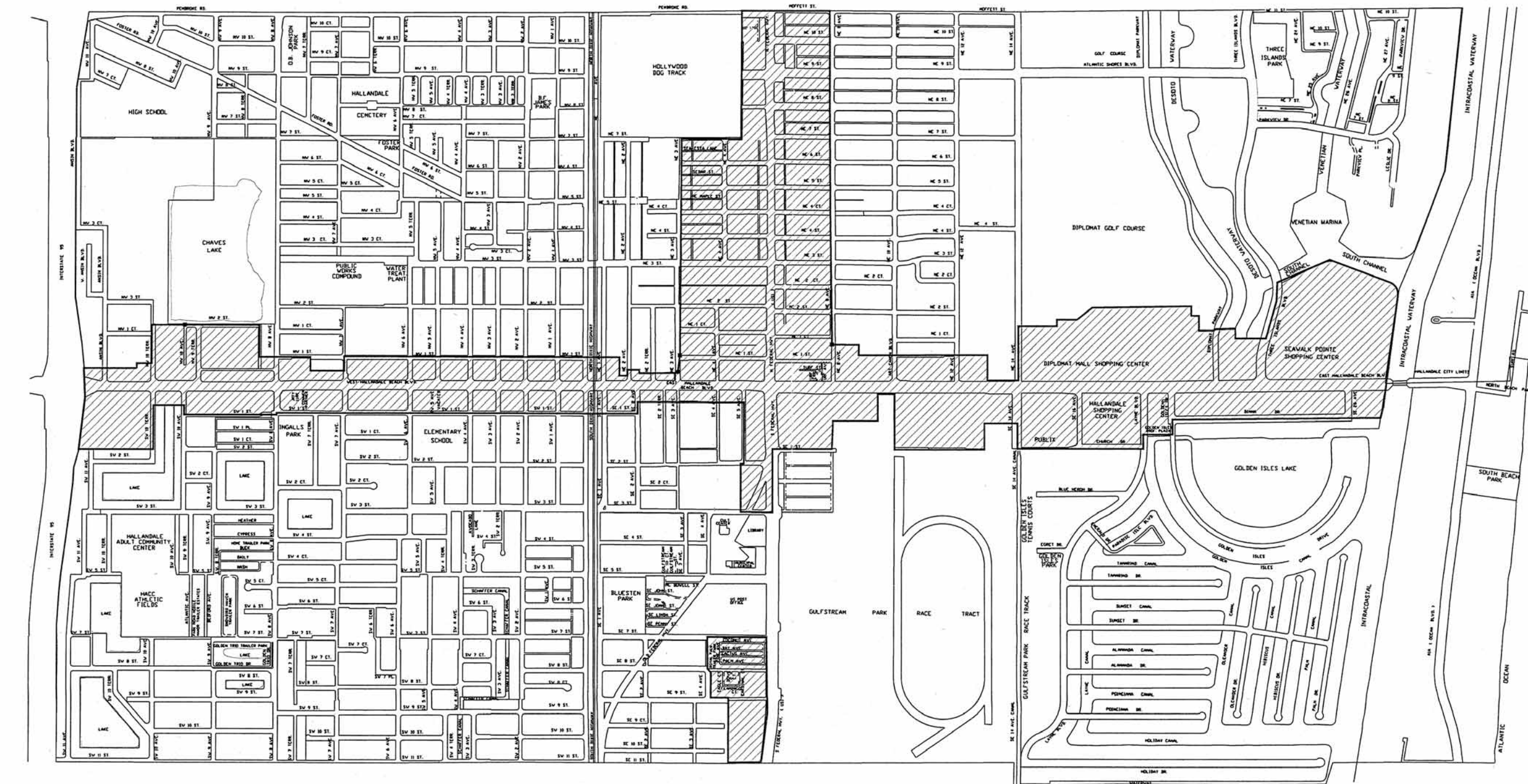
The Planned Redevelopment District (Hallandale Beach Boulevard and Federal Highway Corridors) which are primarily retail and commercial uses, has the greatest potential to redevelop as the City of Hallandale Beach's "main street" or downtown core.

Many existing structures are either prime for rehabilitation or redevelopment. Many sites are now developed with buildings at a considerable distance from the street edge. The Planned Redevelopment District is intended to require redevelopment of the corridors as a pedestrian friendly environment by creating a street definition line. Also, the distance along a street-front that a single use can control should be minimized when developing this district.

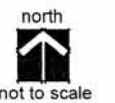
Redevelopment at intersections along this overlay district are encouraged to create pedestrian friendly spaces which address the intersection through the use of open space, building shape and configuration, and landscape treatments.



### 3.0 planned redevelopment overlay district map



 planned redevelopment district



## planned redevelopment overlay district

### 3.2 Buffer Neighborhood Edges

Existing downtown neighborhoods contribute to the overall vitality of the urban core. They are an important part of the mix that creates an active community life in the city. This residential character needs to be considered in the design and planning of adjacent commercial development.

Conflict may occur through the repositioning of commercial development and private homes. Buffering of the neighborhood edges respects their intrinsic value and serves to integrate them into the fabric of downtown. The height, setback, scale, massing and detailing of adjacent commercial buildings should respond to existing homes. Vehicular access to new development should minimize impact on neighborhood traffic flow and avoid disturbing the neighborhood character. Recommendations are as follows:

- a. Projects should be planned to minimize increased use of neighborhood streets.
- b. Where possible, proposed parking, loading, and other vehicular access should occur at mid-block or alley.
- c. Commercial property with frontage on a neighborhood street should limit public access from that street and limit business associated parking on that street so as to avoid significantly altering the residential character and scale of the existing street.



*"Landscape buffer creates division and beauty"*



*"Commercial projects require dense buffering when adjacent to residences"*



## planned redevelopment district

### 3.3 Respect Adjacent Buildings

The City of Hallandale Beach's older buildings are made up of relatively small, wood and masonry buildings with storefronts and large or framed windows. While similar buildings can still be built today, more typical are buildings are taller, more likely to span an entire block, and constructed of smooth stone or concrete panels.

The way buildings are built today can easily leave the older building stock appearing out of place, primarily because of the difference in size, or massing. A very large building next to a very small one will make the smaller seem insignificant unless some gesture is made by the larger.

Awkward repositioning can also be felt when adjacent building differ greatly in their sense of scale, and the degree to which they are articulated on the exterior.

#### Recommendations

- a. Developing in the core area should respond to adjacently existing buildings and avoid creating awkward or incompatible designs.
- b. Compatible designs need not be created through replication, but should reflect a consideration of the scale, material and massing of the adjacent older buildings.
- c. These may include small setbacks of upper stories adjacent to older buildings so that new buildings are consistent with the older buildings at street level.



*"Low level style of older buildings"*



*"Compatible styles and sizes work together"*

## planned redevelopment district

### 3.4 Recycle Existing Building Stock

Existing buildings can provide a sense of continuity through the many memories people have associated with them. Keeping buildings can reinforce unique qualities of a place and so may be more valuable than the cost of the materials alone. It is therefore important that even buildings which are not historically designated, be preserved and integrated into new development when possible.



*"1920's McCrory's building  
still going strong"*



*"Vintage fire house is now  
a Law Office"*



## planned redevelopment district

### 3.5 Create Mixed-Use Development

Creating a variety of uses is critical to having a successful, fully utilized downtown. In America's recent past, the central cores of many cities have been developed primarily as centers for business, inhabited by office buildings, frequently with single tenants. These centers expand and contract with the market for office space, and are disproportionately impacted by economic cycles.

Employees who work in these city centers frequently live outside of it because there are few services available to residents to support residential living. Large numbers leave the core each evening, taking with them the critical population necessary to support a well-rounded downtown area.

A diversity of development downtown can better support residential development and decrease the need for commuting, thus creating a city core where people are able to live, shop and work in a walkable area.

#### Recommendations

- a. At least two different uses per half block are encouraged. Low occupancy uses such as warehouses, storage, and parking will not be counted in the evaluation of mixed-use functions.
- b. Secondary uses should occur at street level and be adequate in size to create a sense of variety at the pedestrian level. As the size of the project increases, the number of street-level secondary uses should increase proportionally.



*"Downtown residences support a well-rounded downtown area"*



*"Retail and office uses combined for convenience"*

## planned redevelopment district

### 3.6 Pedestrian Circulation

Pedestrian related circulation is a priority in the creation of a successfully developed downtown. Tourism, conventions, and daily business all create pedestrian traffic. Currently, vehicular needs dominate the streetscape at the expense of pedestrians. In some areas, discontinuous sidewalks force pedestrians into the street to compete for space.

Major points of interest need better pedestrian connections. Lack of way finding tools such as signs and graphics together with discontinuous and inadequately maintained sidewalks discourage walking.

#### Recommendations:

- a. Appropriately wide sidewalks should be provided from corner to corner along all property lines.
- b. Sidewalks should abut the street curb. Sidewalks extending on and off private property will meet at grade.
- c. Blocks without pedestrian connections should be identified and prioritized for sidewalk construction.
- d. Way finding tools such as specialty pavements, signs and graphics should be provided to facilitate pedestrian movement.
- e. Encourage street vendors, sidewalk cafes, etc. – food attracts pedestrian activity.



*"Pedestrian circulation as a priority"*



*"Specialty pavements lead the way"*

## planned redevelopment overlay district

### 3.7 Design Recommendations

#### **Buildings and Site Redevelopment**

- a. Buildings shall be built to a fifteen (15) foot front lot line (build to line) which is separated from the street proper by a sidewalk and other appropriate streetscape improvement. In areas where adjoining properties have been built to a line less than fifteen (15) feet, new construction shall respect the standard fifteen (15) foot setback.
- b. Where existing improvements are such that a boundary or edge is not provided by a building, improvements shall include street treatments such as street trees, Royal, Washingtonia, Date and Medjool Palms, or landscaped pergolas that are conducive to create a sense of boundary (street definition line) at a maximum of fifteen (15) feet from property line.
- c. Façades on new buildings shall occupy at least sixty (60) percent, but no more than eighty (80) percent of the street frontage on any individual lot. No more than forty (40) percent of the building façade may be opaque.
- d. No more than forty (40) percent of any façade, nor any area of more than 1,000 square feet should be blank wall without windows or decorative treatments.
- e. Off-street parking should not be located between buildings and urban streets.
- f. When garage structures are provided in a commercial or mixed-use development, such shall be designed to incorporate a retail or residential design into the structures facade (wraps) at ground level.

#### **Lighting**

- a. Light fixtures used in the District shall be decorative for new redevelopment within public view and are encouraged throughout the development. The decorative fixtures shall be of a style that compliments the development. Cobra heads are prohibited within a development. Shoebox units may be used but are discouraged at entrances and exits.
- b. Ornamental street lamps should be installed, where feasible in a style similar to the fixtures already existing in the District.
- c. The use of mounted building lights is discouraged, but if used, the light source should not be visible off site.
- d. The color of light sources shall be consistent throughout the project. High-pressure sodium lamps are not permitted.
- d. No more than forty (40) percent of any facade, nor any area of more than 1,000 square feet should be blank wall without windows or decorative treatments.

#### **Fences**

- a. Fences shall be made of wrought iron or aluminum bars with intermittent posts. Masonry walls are also permitted, with no more than twenty (20) percent of the wall opaque. Chain link fences are privacy wood fences are prohibited along the corridors. Sharp projections, barbed wire or other hazardous materials are not permitted as any part of a fence or wall. Wrought iron and aluminum bar fences shall be either black, white or match the color of the building. Masonry wall shall match the building color to reflect Florida coastal themes. Colors shall be muted tones such as soft yellow, cream, granite or rose.
- b. The gates shall not be chain link or wood, but must be aluminum or other durable material.
- c. Chain link or wood fences are prohibited in this district, and any existing chain link or wood fences shall be removed.

fashion row overlay district

**Section Four**

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## fashion row overlay district

### 4.0 Intention Statement

The Fashion Row Overlay District represents a portion of the commercial corridor east of Dixie Highway and north of Hallandale Beach Boulevard. This area is intended to encourage and promote construction specialty trades that can interact amongst each other. The establishment of retail and warehouse with a fifty/fifty mix will cater specifically to the fashion, home improvement, and building industry. The city's desire is to attract businesses such as: paint, millwork, plumbing, ironwork, upholstery and home improvement design. It is an appendage to the Planned Redevelopment District (Hallandale Beach Blvd.) and has good potential for future infill re-development.

The district's close proximity to Interstate 95 and Dixie Highway should encourage the clustering of design oriented businesses that can draw patronage from neighboring cities. As the district develops, the placing of buildings at the street with vehicular access from the side and rear is the existing primary distinction of the district and should be encouraged. The development of the railroad corridor as a parkway on the western edge of this district is encouraged.

This manual sets forth required design standards and design suggestions for buildings and their on-site surroundings. All new construction and renovation of existing properties, as distinguished from repair work or maintenance, shall comply with these guidelines. Specifically, these design standards and provisions shall apply to new (completely replaced) paint, roofs, window tinting, awnings, signs, shutters, sidewalks and other design elements regulated herein. For example, touching up the paint on a wall face is considered repair or maintenance, the complete repainting of a wall face is considered new or replacement. These standards are a supplement to Section 32-171, Fashion Row Overlay District of the Zoning and Land Development Code.

These design standards are divided into two sections. Section 4.2 contains design standards for the entire Fashion Row Overlay District. Section 4.4 contains additional standards applicable only to a one-block area of the Fashion Row District designated as RM-18 zoning. The supplement standards for this one-block area retailored for commercial and mixed-use commercial/residential uses in existing residential structures.

The Fashion Row Design Guidelines are administered by the City of Hallandale Beach Development Services Department. Appeals of staff decision and modification of design standards are provided for by Section 32-171 of the Zoning and Land Development Code. Where these design standards conflict with the Fashion Row Overlay District, Zoning Code or other City regulations, these standards shall apply within the Fashion Row Overlay District.



## fashion row overlay district

## 4.2 Guidelines Applicable to Fashion Row District

### **Landscaping** (See Section One – 1.3)

#### **Planters and Sillboxes**

Planters offer an effective method of landscaping a sidewalk area when tree planting is not possible. Planters are planting pots that can be placed wherever space permits, and may contain small trees or other plant material. Note that planter locations are subject to minimum pedestrian clearance of four (4) feet. Another type of planter is the sillbox, which is attached to a building façade in a square or rectangular shape, and usually contains flowering plants or vines.

Planters and sillboxes are encouraged throughout the Overlay District especially in areas where pavement precludes grass, hedges, trees, and landscaped areas.

When utilized, planters shall be a material capable of resisting corrosion and decay and secured to withstand high winds. Clay and cement pots are encouraged and plastic planters are prohibited. Planters can be placed on private property and right-of-ways provided adequate pedestrian or automotive clearance is maintained as specified in the Zoning and Land Development Code. The location of planters in the right-of-ways is subject to City Manager approval. The City of Hallandale Beach reserves the right to remove planters obstructing public ways.

Sillboxes could be made of coral stone, stone or painted concrete, and attached to the building. Sillboxes may project a maximum two and one-half (2½) feet into a required yard, provided that at least three (3) feet of pedestrian clearance remains between the sillbox and any property line, parking facility, structure, or other obstruction.

#### **Street Trees**

Where street trees are planted within the sidewalk, plant material can surround the base of the tree. Property owners shall be responsible for maintenance of plant material, planters and sillboxes in order to reflect a well-maintained district.



*"Sidewalk planters offer an effective method of landscape"*



*"Potted plants and sidewalk planters create a garden setting."*



*"Street trees planted within the sidewalk"*

## fashion row overlay district

**Fences** (See Section One – 1.0)

**Outdoor Restaurant Seating** (See Section One – 1.12)

**Dumpster Enclosures** (See Section One – 1.4)

### **Building Facades and Storefronts**

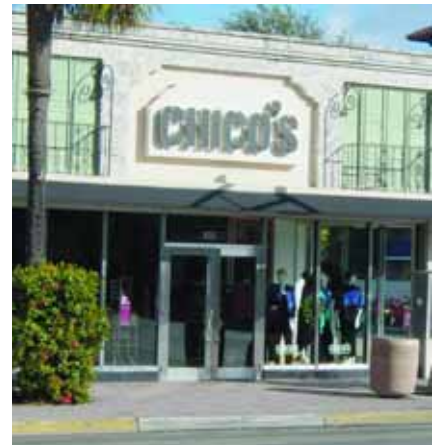
Coordination of some façade components in the Fashion Row District would help create a visual perception among shoppers that the individual buildings are part of a special shopping district, while still allowing individual buildings to retain their own identity. Individual design components, such as building colors, windows, doors, storefronts, signage and awnings, shall be coordinated to create a harmonious effect.

First floor facades shall be comprised of at least sixty (60) percent glass for the first eight (8) feet above grade. This shall not apply to existing residential buildings on N.E. 2nd Avenue. The bottom windows shall begin between one (1) and two (2) feet above sidewalk elevation.

### **Windows**

Windows (office or storefront) are among the most important elements in establishing an economically vibrant commercial district. Windows provide the opportunity to display merchandise and let the public know of its availability. Existing display windows shall not be eliminated or decreased in size.

Dark or reflective tinting and reflective coatings are prohibited. Minimal tinting to meet energy codes, or other regulatory requirements, may be acceptable, if compatible with the architectural character of the building.



*"Merchandise displayed clearly"*



## fashion row overlay district

### Awnings and Canopies

Awnings and canopies provide shelter from rain, shade from the sun, a location for limited signage and enhance a building's appearance. When multiple awnings are attached to a building, awnings shall be of identical height and depth. Awning depth and height should be as consistent as possible with awnings of adjacent buildings.

In the case of a building containing multiple storefronts, it is preferable to have one continuous awning the full length of the building. Exceptions may be made when the architectural features of a building differentiate separate stores. If it is necessary or desirable for individual stores or windows to have their own awnings, the separate awning shall have the same form, fabric and color.

Fabric awning/canopies can be painted. The size of awnings shall be proportional to the scale of a building and the surrounding streetscape.

Signs on awnings/canopies shall be consistent with signage regulations.

### Signage

Signage provides the District with a unique identity and can create a unifying character. All signage shall be designed for three basic functions: individual business identity, direction and location of activity, and overall District identity.

Within one building containing different businesses, signage shall be of the same type of sign. Property owners shall specify which type of sign (ex: cabinet, wood, painted wall, channel letter, etc.) may be installed by the various tenants. Existing tenant's signage shall not be required to comply with the designated type of signage, however, new signs shall be required to comply; the purpose being is to project a sense of unity and relationship among the businesses.



*"Consistent awning style creates theme"*



*"Signage identifies district"*

### fashion row overlay district

#### Window Displays

One of the most important impressions a business makes on the pedestrian customer is with the window display. The methods in which merchandise is shown or, in some cases, not shown provides the customer with a mass of information and impressions about the store and its merchandise. The following information is provided for the applicant's consideration.

The purpose of a window display includes capturing the attention of those passing by, establishing a positive and professional image for the business, informing the potential customer of the merchandise available in the store, and enticing them to make a purchase. Simply putting merchandise in a window without paying attention to its display, sends a message to the potential customer that the merchandise is uninteresting, and the store is run in an unprofessional manner.

The merchandise selected for a window display is a sample of what can be found in the store. It is not necessary to display all available merchandise in the window at one time.

Window displays should be changed frequently. Merchandise displays should be changed weekly or bi-weekly. The overall display (background, props, lighting) can be used through several merchandise changes, but some modification should occur periodically. Displays which remain unchanged are soon taken for granted and items may become faded and dusty and stop attracting customers.

#### Lighting

Proper lighting can be one of the most powerful methods of establishing a business image with the public during evening hours. Even if a store is closed during the evening, lighting of the building, signs and windows is an effective marketing tool. Lighting is also important in mixed-use commercial/residential buildings. It increases visibility, enhances the views for residents and improves security.

Within a typical storefront, those elements which need illumination include signs, entrances, window display, and the interior. The levels of illumination for each of these areas should be varied. It is unnecessary to provide intense, glaring illumination to attract attention to the storefront. In some cases, lighting levels lower than adjacent businesses, but with carefully placed highlights, are more dramatic and attention getting. In residential/commercial use projects and mixed residential/commercial use projects, light levels should clearly denote entrances, walkways and other circulation systems, as in parking areas.



*"Windows effectively display merchandise"*



*"Lighting of building is an effective marketing tool"*

### fashion row overlay district

In addition to space, sign, and storefront display lighting, it is often desirable to light the structure itself. Many buildings possess attractive and unique architectural details which should be enhanced with discreet architectural/decorative lighting, indirect lighting and hidden spotlights.

Alleys and rear/side delivery areas shall have lighting, which remains on between dusk and dawn, but shall not cause glare on residential properties.

Decorative lighting of landscape and façade features is encouraged.

#### **Roofs**

Mansard roofs are not permitted to be rebuilt, and shall be removed when in disrepair, unless such a roof is integral to the building structure. The existing building wall below any removed mansards shall be repaired and painted to either match the existing walls or the approved color palette.



*"Decorative lighting in landscape features"*



*"Mansard roofs are not permitted"*

## fashion row overlay district

### **Shutters and Security Grills**

Hurricane shutters and security grills are important for building safety. The following guidelines can make the shutters or grills less noticeable, so they do not attract detract from a building's appearance. New shutters and security grills shall comply with these provisions. This regulation shall only apply to existing grills and shutters where specifically stated herein.

Roll-up or accordion shutters are permitted on the ground floor fronting a public street when constructed of a see-through, non-solid grate material. The same type shutter shall be used throughout the building. The casing for the grilles and shutter tracks shall be painted to match the building color and shall not damage or obscure architectural, historic or decorative material. Existing casing, shutters, and security grills must be painted to match building color when the façade is painted.

Roll-up or accordion shutters of a solid nature, meeting hurricane protection requirements may be installed on upper floors, if all windows are included and the same shutter is used on all windows. These shutters may also be used on ground floor windows, which do not front upon a public street.

Security bars are not encouraged, but if installed on the inside of windows, must be painted to match the color of the window surrounds.





## fashion row overlay district

### 4.4 Guidelines Applicable to RM-18 District

This plan illustrates the general intent of the N.E. 2nd Avenue plan (RM-18 district). The district ordinance allows for the conversion of existing residential uses to commercial use. The street will operate as two-way north-south with parallel parking. Individual property owners are responsible for building their segment of sidewalk if conversion to commercial use is proposed. Design Guidelines are provided to illustrate the intent of the Fashion Row District Ordinance and serve as a guide to development and revitalization.

#### **Landscaping** (See Section One - 1.3)

#### **Signage**

Size, number, type and location of signage permitted in this area of the Fashion Row District are regulated by the above referenced subsection of the Zoning and Land Development Code. Backlit and "lightbox" type cabinet signs are specifically prohibited.

#### **Porches**

Porches in the RM-18 District are encouraged and allowed to project up to ten (10) feet from the property line. Where porches are added to existing structures, the square footage of the porch area shall not count toward floor area when calculating parking requirements.

Porches shall be of substantial construction and constructed according to City ordinances and the South Florida Building Code.

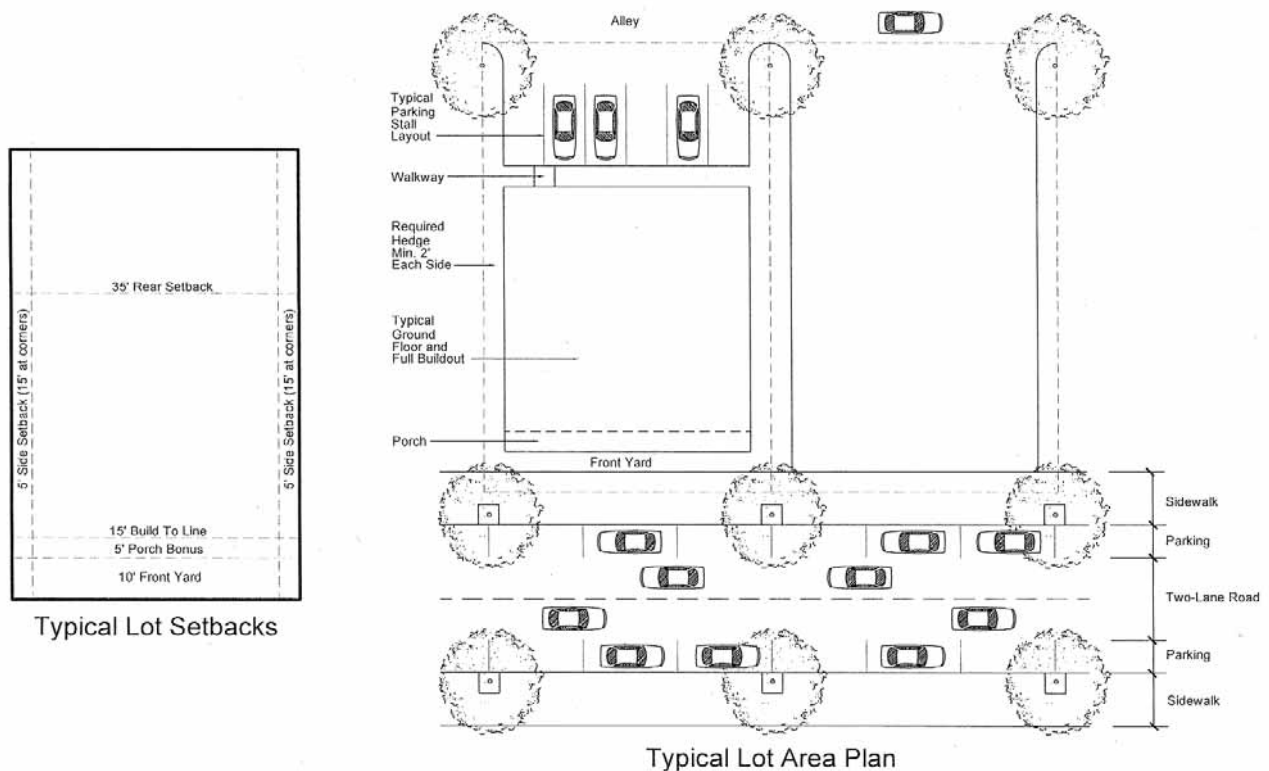
Porch Railings and posts shall be of wood or wrought iron and painted to compliment the existing building color or shall be consistent with color palette.



*"Porch railings and posts painted to complement building color"*

## fashion row overlay district

This concept plan illustrates the intent of the RM-18 District along N.E. 2nd Avenue. It is important to note that the number of parking spaces provided on the site will determine the maximum allowable commercial floor area and/or maximum dwelling units. Build out of a property could not be achieved with six parking spaces as shown on this concept plan.



north dixie corridor overlay district

**Section Five**

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5.2	Guidelines Applicable to North Dixie Corridor Overlay District	80





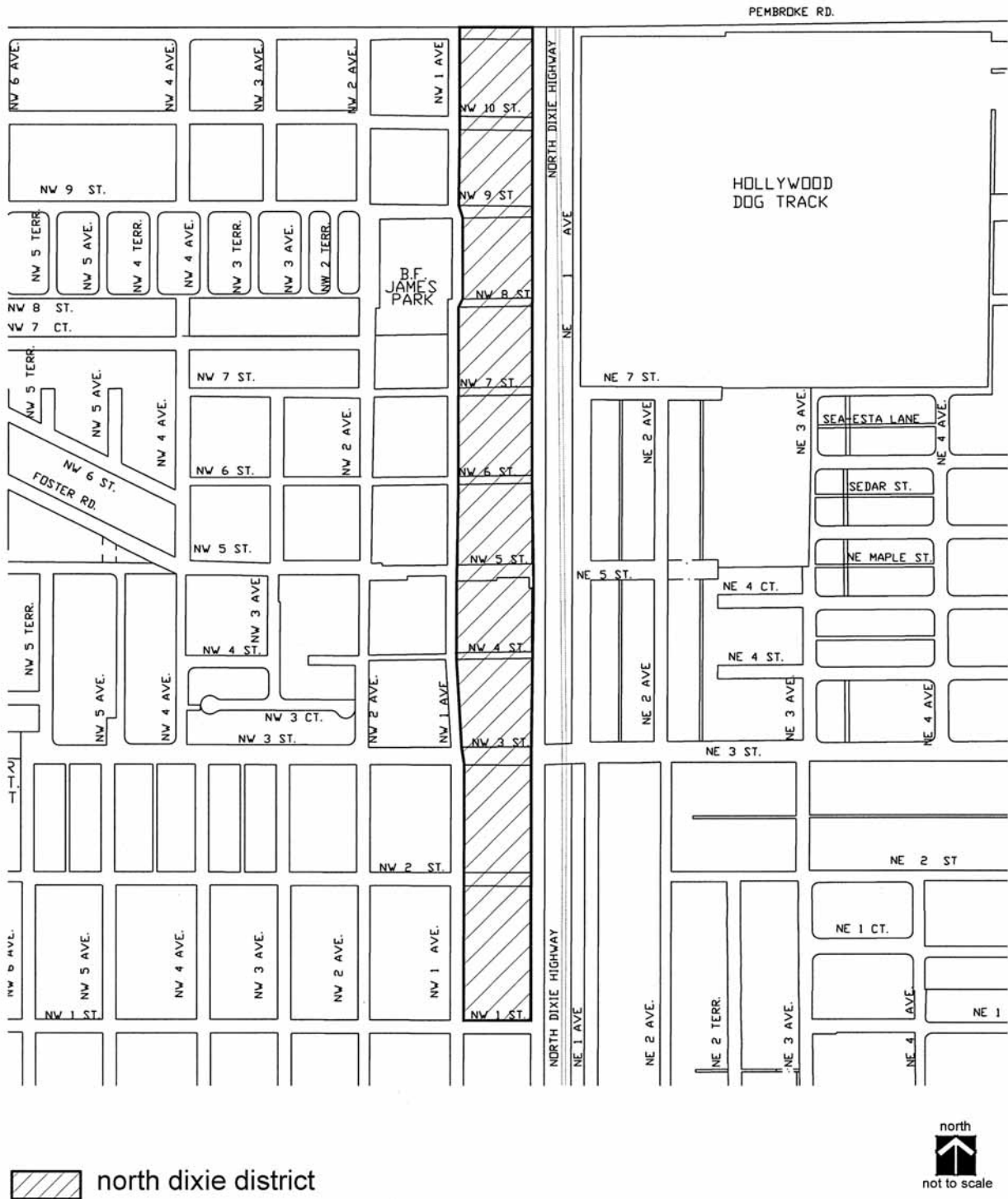
## north dixie corridor overlay district

### 5.0 Intention Statement

The North Dixie Corridor Overlay District is located on the western edge of Dixie Highway, which serves as a north/south corridor. A substantial green space is located between the one-way south bound lanes of Dixie Highway, and the western side of the rail line, on the western edge of the railroad right-of-way. The railroad corridor is heavily grassed and landscaped adjacent to the roadway, which presents an opportunity to be a pedestrian amenity, which supports the important urban concept of putting the pedestrian first. The natural park-like ambiance should be enhanced and encouraged. It also assists redevelopment to provide an attractive corridor and a tree-lined streetscape compatible with the neighborhoods.

The North Dixie Corridor Overlay District is intended to encourage the redevelopment of local businesses, the development of new businesses and provide a comfortable transition and buffer from Dixie Highway to the residential area to the west. The overlay district currently consists of local service businesses with an emphasis on automotive oriented uses. Buffering of these business uses from the residential uses to the west N.W. 1st Avenue, is required for all new developments. At this time many businesses provide back-out parking along Dixie Highway. It is the desire of the city that redevelopment of these properties will eliminate this unsafe parking scenario. Placing buildings at the street with vehicular access from the side and rear will serve to reduce street access and create a more pedestrian friendly environment.

## 5.1 north dixie corridor overlay district map



## north dixie corridor overlay district

## 5.2 Guidelines Applicable to North Corridor Overlay District

Section 32-172 of the City of Hallandale Beach Zoning and Land Development Code has designated an overlay district for the North Dixie Highway Corridor, providing for district boundaries; establishing uses permitted in the district; providing for landscape buffer requirements; establishing access requirements; providing for street trees; mandating uniform signage and allowable fence types. It also creates a redevelopment area modification process, providing for conflict and severability.

### District Boundaries

There is designated as the North Dixie Corridor Overlay District those lands generally and approximately bounded by Pembroke Road to the north, the F.E.C. Railroad tracks to the east, N.W. First Street on the south, and N.W. First Avenue on the west.

All uses within the North Dixie Highway Overlay District shall comply with the applicable regulations, the supplemental regulations of Section 32-172 of the Zoning and Land Development Code, and these design guidelines.

### Permitted Uses

All uses specifically permitted by this provision and the underlying zoning district shall be permitted within the North Dixie Highway Overlay District. Other uses, similar in nature to the uses permitted in the underlying zoning district but not specifically permitted therein, may be permitted within the North Dixie Highway Overlay District if not prohibited by this provision.

#### Conditional Uses:

Automotive paint and body shops, including all uses accessory to automotive sales, provided that all activities are conducted within an enclosed, soundproof building and that no outside storage of junk vehicles or vehicle parts shall be permitted. All activities shall be such that no service bay shall be oriented toward North Dixie Highway.

### Prohibited Uses

The following uses are prohibited within the North Dixie Overlay District:

- a. Churches, Synagogues or houses of worship of any kind.
- b. Paint and body repair activity or the like which generates noise, odor, fumes or dust shall be permitted within 50 feet of any residential or community facility zoned property or use.



*"Established businesses  
in the district"*

## north dixie corridor overlay district

### Landscape Buffers

Within the overlay district, a buffer area shall be provided adjoining N.W. First Avenue when a residentially or community facility zoned property or uses adjoins or is separated by a public right-of-way from a commercial use. The buffer area shall include a sidewalk, a densely landscaped buffer strip and a masonry wall that is incorporated into the design scheme of the project. Minimum specifications for the buffer area shall be set forth below.

- a. Landscape Buffer Strip: An eight (8) foot landscaped buffer strip shall be provided along the exterior of the masonry wall. This buffer strip will be landscaped with dense hedges and trees, and plant materials shall be subject to the vision clearance requirement provided in Section 32-332.

### Landscape Area Improvements

All landscaped areas shall include xeriscape plant materials. All off-site abutting swale and buffer areas shall be sodded. Hedges and street trees shall be provided in the required buffer strip in accordance with the following provisions:

- a. Xeriscape plant materials: A minimum of fifty (50) percent of the required plant materials shall be classified as xeriscape, and shall be included in required landscape plans and selected in accordance with city approved plant material lists such as Xeriscape Plant Guide II, published by the South Florida Water Management District, and identified as:
  - (1) Very-drought tolerant
  - (2) Non-invasive
  - (3) Medium to fast growth rate
  - (4) Sub-tropical to hardy climate designation
- b. Provision of irrigation system: The use of irrigation support systems for all pervious areas shall be provided. Automatic operating, underground or zoned, water-efficient irrigation systems shall be installed for all sodded areas and must provide one hundred (100) percent coverage.



*"A buffer area with sidewalk"*



*"50% xeriscape plantings are required"*



## north dixie corridor overlay district

- c. Hedges: All buffer areas shall contain a minimum hedge of plants at least two (2) feet in height immediately after planting and shall be maintained at a maximum height of three (3) feet. Hedges shall be planted with branches touching, so to create a solid screen with no visual spaces between plants at time of installation.
- d. Street Trees: The planting of street trees shall be required within the North Dixie Highway Overlay District. The following criteria shall apply to the planting of street trees within the overlay district in required buffer strips:
  - (1) Street trees shall not be counted toward the minimum number of trees required by Section 32-381.
  - (2) Street trees shall be a minimum of twenty (20) feet in height with a minimum diameter of three (3) inches at a point four (4) feet above ground level at time of planting and having trunks which can be maintained in a clean condition for over six (6) feet of clear wood measured from ground level.
  - (3) Street trees shall be spaced, center to center, at a minimum of one (1) tree per twenty-five (25) linear feet of street frontage, exclusive of accessways.
  - (4) Street trees shall be of non-invasive root species and shall be designated as street or shade trees in the City's approved plant material list.
  - (5) The minimum height required in this subsection for street trees may be modified by the Director of Development Services without penalties, in instances where the installation of the trees would interfere with overhead utilities, so long as the minimum number and spacing of trees is maintained.



*"Continuous hedge creates a solid screen"*



*"Street trees every 25' for a shaded canopy"*

### Access Requirements

All development proposals shall provide shared access easements wherever feasible to adjoining properties and projects. Interconnection of developments shall be required as necessary to minimize the number of curb cuts and driveways onto local streets and particularly North Dixie Highway and N.W. First Avenue. New or reconstructed access to commercial projects within the overlay district shall be restricted to North Dixie Highway and/or shall not be located a distance greater than 130 feet west of North Dixie Highway along local streets. No new or reconstructed access shall be permitted to N.W. First Avenue except for emergency purposes.

## north dixie corridor overlay district

### Signage

No signs shall be permitted along N.W. First Avenue within the overlay district except for traffic control and directional signs, address signs, and dedication plaques not to exceed six (6) square feet in gross sign area. Businesses with frontage along North Dixie Highway are permitted one wall sign per business establishment. Businesses with over 100 feet of frontage are permitted one free-standing sign. All signs must meet criteria set forth in section 32-605(d).

### Walls and Sidewalks

- a. Sidewalk: A five (5) foot sidewalk shall be provided along any street right-of-way or on private property by easement dedication if the right-of-way is of insufficient width.
- b. Walls: A masonry wall shall be provided adjoining the required landscape buffer strip. No barbed wire extensions shall be permitted on any walls or fences within the overlay district.
- c. The building line may coincide with the edge of the N.W. First Avenue landscaped buffer strip, in lieu of the required wall, subject to the following:
  - (1) The building wall shall be finished in accordance with the standards set forth for masonry walls.
  - (2) No overwall or roof drainage shall be permitted.
  - (3) A pitched roof shall be provided above the finished wall face.
  - (4) Rooftop equipment shall be screened.

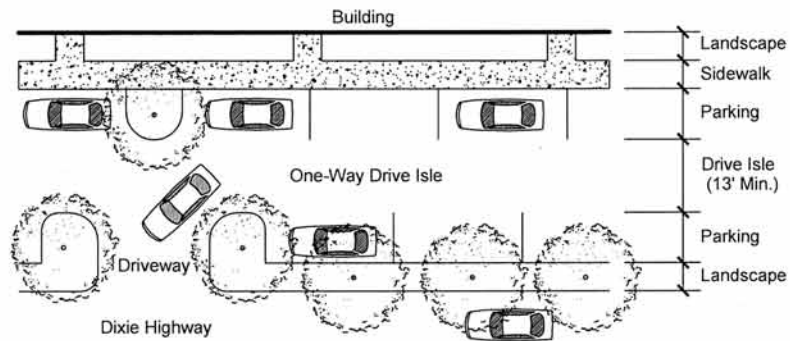


*"Signage adds to business atmosphere"*

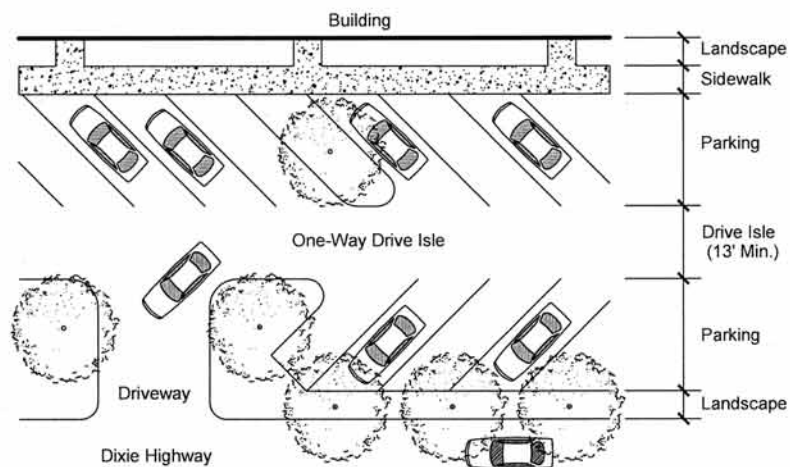


*"Dense Landscaping to enhance masonry wall"*

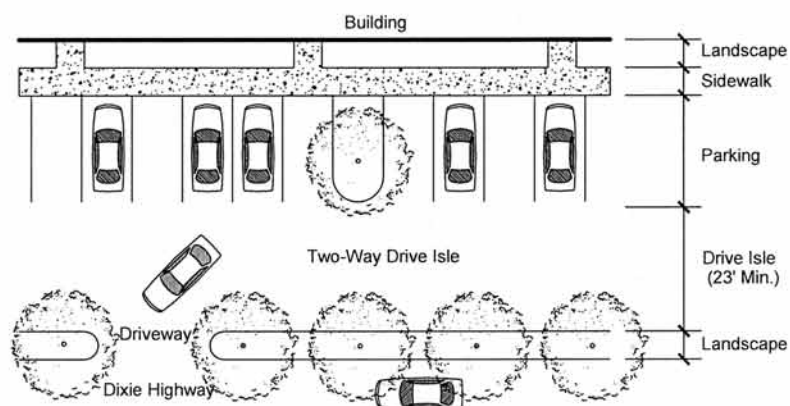
north dixie corridor overlay district



Parallel Parking



Angled Parking



90 Degree Parking

Acceptable Off-Road Parking Alternatives



## south dixie corridor overlay district

### **Section Six**

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6.1	South Dixie Corridor Overlay District Map	87
6.2	Guidelines Applicable to South Dixie Corridor Overlay District	88





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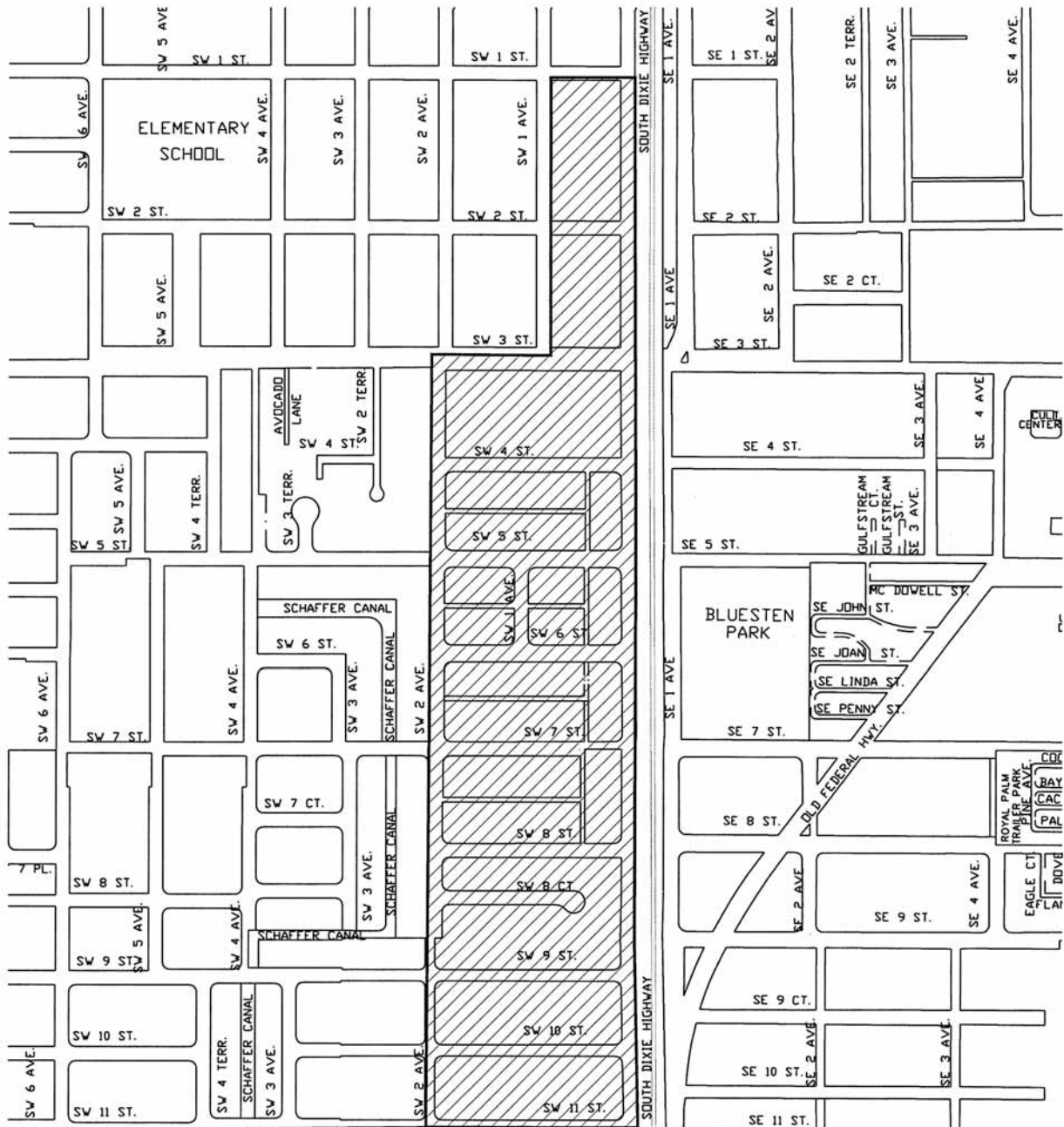
## south dixie corridor overlay district


### 6.0 Intention Statement

The South Dixie Corridor Overlay District is located on the western edge of Dixie Highway, which serves as a north/south corridor. As in the North Dixie Corridor Overlay District, a substantial green space is located between the one-way south bound lanes of Dixie Highway, and the western side of the rail line, on the western edge of the railroad right-of-way. The railroad corridor is heavily grassed and landscaped adjacent to the roadway, which presents an opportunity to be a pedestrian amenity, which supports the important urban concept of putting the pedestrian first. The natural park-like ambiance should be enhanced and encouraged. It also assists redevelopment to provide an attractive corridor and a tree-lined streetscape compatible with the neighborhoods.

The South Dixie Corridor Overlay District is intended to encourage the redevelopment of local businesses, the development of new businesses and provide a comfortable transition from retail and service establishments. It also acts as a buffer from Dixie Highway to the residential area to the west. The overlay district currently consists of local service and retail businesses. At this time many businesses provide back-out parking along Dixie Highway. It is the desire of the city that redevelopment of these properties will eliminate this unsafe parking scenario. Placing buildings at the street with vehicular access from the side and rear will serve to reduce street access and create a more pedestrian friendly environment.

## 6.1 south dixie corridor overlay district map



 south dixie district



## south dixie corridor overlay district

### 6.2 Guidelines Applicable to South Dixie Corridor Overlay District

Ordinance No. 2002-03 of the City of Hallandale Beach Code of Ordinances has created an overlay district for the South Dixie Highway Corridor, providing for district boundaries; establishing uses permitted in the district; providing for landscape buffer requirements; establishing access requirements; providing for street trees; mandating uniform signage and allowable fence types. It also creates a redevelopment area modification process, providing for conflict and severability.

#### **District Boundaries**

For the purpose of defining the boundaries, the South Dixie Highway Overlay District is divided into two sections: north and south of S.W. 3rd Street. The north sector is bounded by S.W. 1st Street to the north; South Dixie Highway to the east; S.W. 1st Avenue to the west and S.W. 3rd Street to the south. The boundaries of the south sector is bounded by S.W. 3rd Street to the north; South Dixie Highway to the east; S.W. 2nd Avenue to the west; and S.W. 11th Street to the south.

All uses within the South Dixie Highway Overlay District shall comply with the applicable regulations, the supplemental regulations of Ordinance 2002-03, and these design guidelines.

#### **Permitted Uses**

All uses specifically permitted by this provision and the underlying zoning district shall be permitted within the South Dixie Highway Overlay District. Other uses, similar in nature to the uses permitted in the underlying zoning district but not specifically permitted therein, may be permitted within the South Dixie Highway Overlay District if not prohibited by this provision.

Retail and service establishments limited to:

- a. Restaurants provided that:  
Freestanding restaurants must have a minimum of 2,000 square feet of gross floor area.

#### **Prohibited Uses**

The following uses are prohibited within the South Dixie Highway Overlay District:

- a. Automobile Sales and Display (used)
- b. Automobile Repair and Service
- c. Automobile Parts and Accessory Sales
- d. Boat Sales and Displays
- e. Service Stations including Accessory Gas Sales
- f. Car Washes



## south dixie corridor overlay district

### Landscape Buffers

The buffer area between a commercial use in the overlay district and a residential use separated by the right-of-way shall be at least ten (10) feet wide and contain dense landscaping capable of effectively screening the two uses. Screening shall be composed of landscaped masonry walls that are incorporated into the design scheme for the project. The buffer strip shall be provided along the exterior of the masonry wall. Screened walls shall be eight (8) feet in height and shall be subject to the vision clearance requirement of Chapter 32-384, Zoning and Land Development Code. The required landscape buffer strip shall be planted with a two (2) foot high hedge abutting the wall and fifteen (15) foot shade trees spaced twenty-five (25) feet on center for the length of the wall.

### Access Requirements

All development proposals shall provide shared access easements wherever feasible to adjoining properties and projects. Interconnection of developments shall be required as necessary to minimize the number of curb cuts and driveways onto local streets and particularly South Dixie Highway.

### Signage

- (1) Uniform signage: Signage on buildings with multiple storefronts shall be of the same type (such as box sign, channel lettering, molded lettering). Painted wood wall signs are prohibited within the overlay district.



*"Landscape screening incorporated in the design scheme"*



*"Uniform signage on buildings with multiple storefronts"*



## south dixie corridor overlay district

### Walls, Fences and Sidewalks

- (1) Masonry walls shall be painted to blend in with the overall architectural design scheme of the development proposal. All masonry walls shall be finished on both sides so that no CBS course is visible.
- (2) Barbed wire on fences and walls are prohibited in the overlay district. Barbed wire shall be removed within one (1) year of adoption of this ordinance.
- (3) Galvanized chain link fences along South Dixie Highway and public rights-of-way are prohibited on commercially zoned properties. Walls and fences along such rights-of-way shall not exceed six (6) feet in height. Existing chain link fences shall be removed or replaced with permanent fencing within one year of adoption of this ordinance.
- (4) Vinyl-Clad chain link fencing, masonry walls or aluminum picket fencing are permitted. When masonry walls are used other than as specified, a minimum of a two (2) foot landscape area planted with hedges and trees shall be required on the exterior of the wall.
- (5) A five (5) foot sidewalk shall be provided along any street right-of-way or on private property by easement dedication if the right-of-way is of insufficient width.

### Landscape/Screening Treatment

Building wall areas more than twenty (20) feet in length shall be interrupted by landscaped areas and planted with a minimum of one tree fifteen (15) feet per every twenty (20) linear feet of wall area to break up the monotony of the wall face.



*"Aluminum fencing adds to the Landscape buffer"*



*"Trees break up monotony of a Long Wall"*

## south dixie corridor overlay district

### Provision of Street Trees

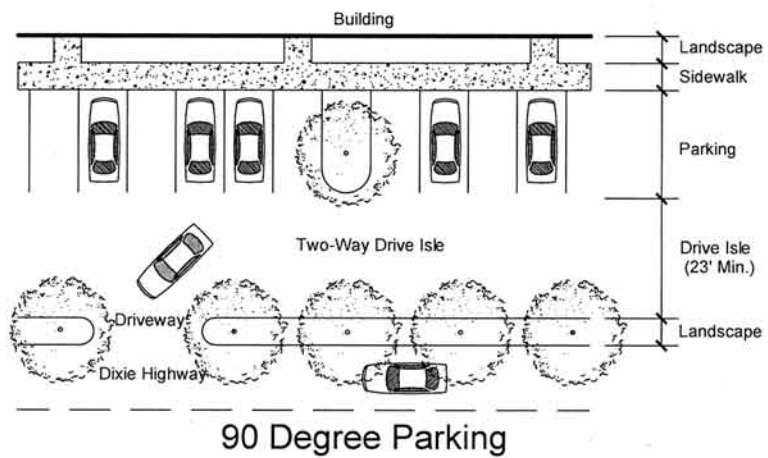
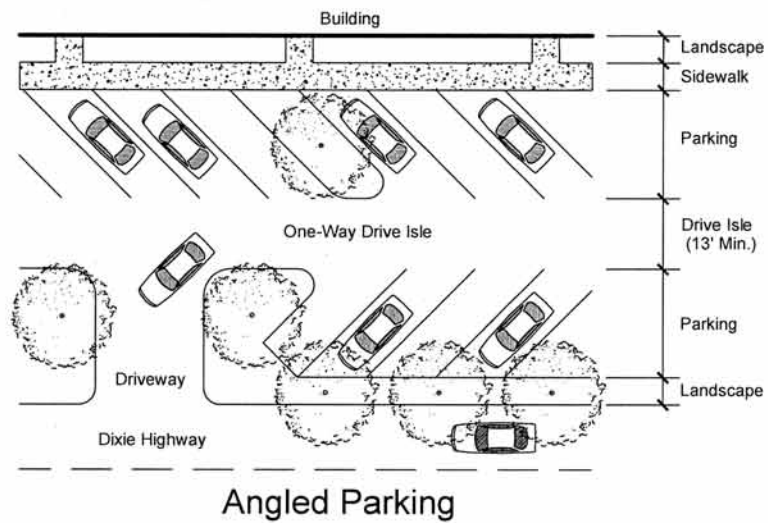
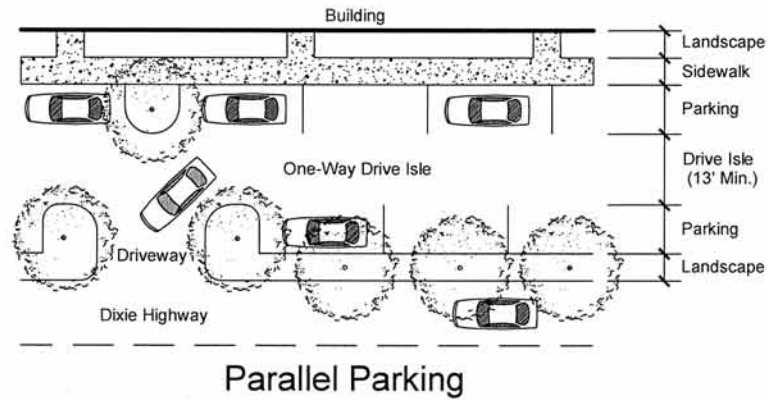
The planting of street trees shall be required within the South Dixie Highway Overlay District. Development proposals shall provide for installation of street trees in conformance with the following criteria:

- (1) Street trees shall not be counted toward the minimum number of on-site trees required by Chapter 32 of the Zoning and Land Development Code.
- (2) Street trees shall be a minimum of twenty (20) feet in height with a minimum diameter of three (3) inches at a point four (4) feet above ground level at time of planting and having trunks which can be maintained in a clean condition for over six (6) feet of clear wood measured from ground level.
- (3) Street trees shall be spaced, center to center, at a minimum of one (1) tree per twenty-five (25) linear feet of street frontage, exclusive of accessways.
- (4) The minimum height and specified street trees for major transportation corridor standards contained herein for street trees may be modified by the City Manager without penalty, in instances where the installation of the trees would interfere with overhead utilities, so long as the minimum number and spacing of trees is maintained.
- (5) Street trees shall be of non-invasive root species and shall be designated as street or shade trees in the City's approved plant material list. The designated street tree for South Dixie Highway is the Royal Palm. The designation for all other local street trees shall be Mahogany trees.



*"Landscaped street treatment"*

south dixie corridor overlay district



Acceptable Off-Road  
Parking Alternatives

## foster road corridor overlay district

### **Section Seven**

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7.1	Foster Road Corridor Overlay District Map	95
7.2	Guidelines Applicable to Foster Road Corridor Overlay District	96





## foster road corridor overlay district map

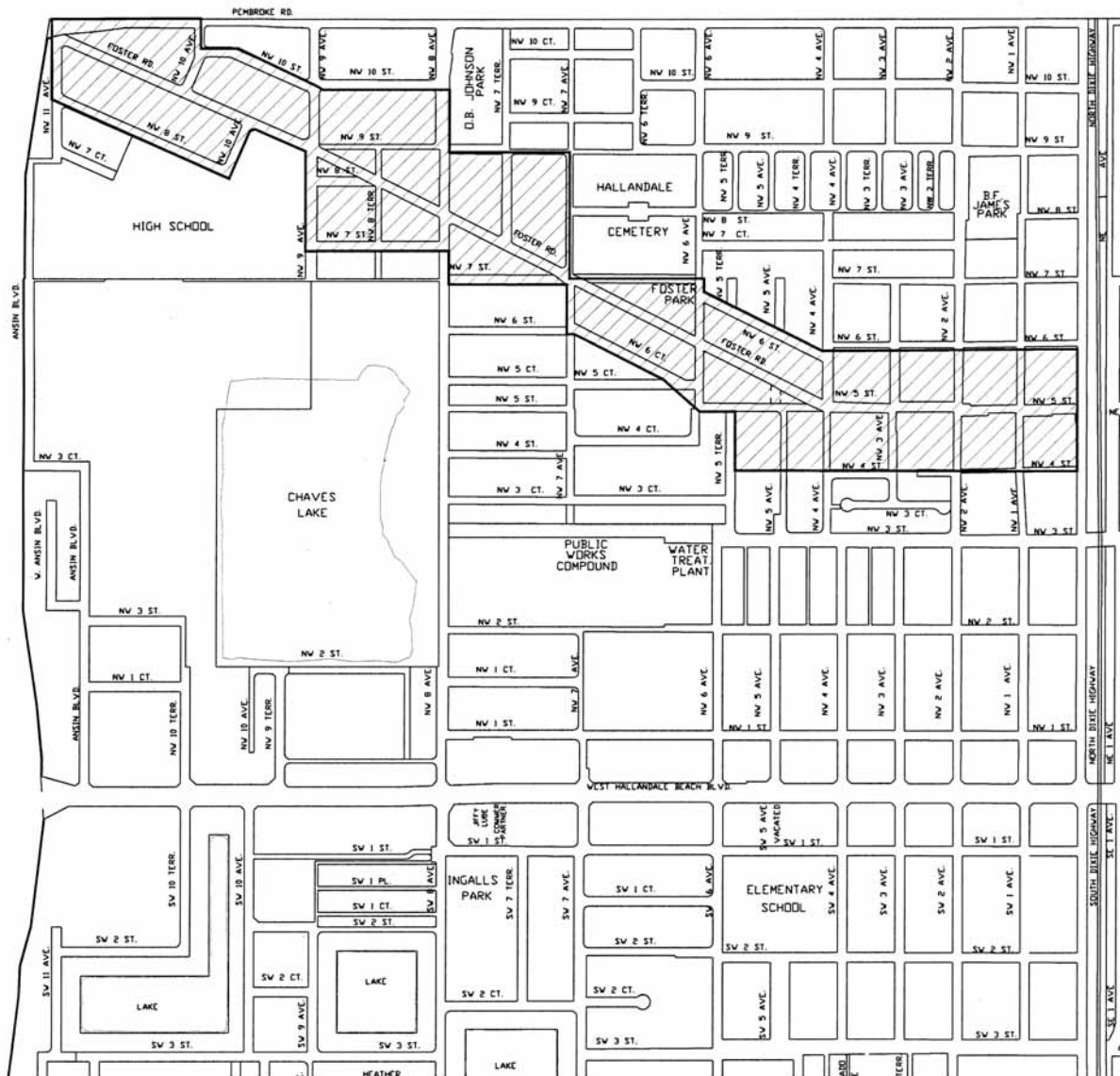
### 7.0 Intention Statement


The Foster Road Corridor Overlay District is located south of Pembroke Road and west of Dixie Highway. The district is parallel to a diagonal boulevard containing both businesses and housing.

The intention of this overlay district is to guide small scale neighborhood compatible businesses to redevelop the streetscape of Foster Road. Recommendations for the overlay district include low-rise mixed-use structures containing retail/commercial uses predominantly at street level, with residential uses above. The retail/commercial businesses should be supportive neighborhood services existing in harmony with multi-family housing. Pedestrian access should be a primary consideration when redeveloping within this district.

This neighborhood redevelopment should provide a comfortable transition into the local neighborhood to the north and south.

## 7.1 foster road corridor overlay district map



 foster road district



## foster road corridor overlay district

### 7.2 Guidelines Applicable to Foster Road Corridor Overlay District

#### **District Boundaries**

See 7.1 Foster Road Overlay District Map

All uses within the Foster Road Overlay District shall comply with the applicable regulations, the supplemental regulations of Ordinance 2003-15, and these design guidelines.

#### **Permitted Uses**

All uses specifically permitted by this provision and the underlying zoning district shall be permitted within the Foster Road Overlay District. Other uses, similar in nature to the uses permitted in the underlying zoning district but not specifically permitted therein, may be permitted within the Foster Road Overlay District if not prohibited by this provision.

Retail and service establishments limited to:

- a. Restaurants provided that:  
Freestanding restaurants must have a minimum of 1,000 square feet of gross floor area.

#### **Uses Permitted Conditionally**

- a. All uses specifically permitted by the underlying zoning districts as a conditional use are permitted in accordance with established procedures for a Conditional Use Permit, unless the use is prohibited within these provisions.
- b. Townhouse development and multi-family residential uses of sites of not less than 10,000 square feet, including mixed residential and commercial in the same structure on commercial land use designated and zoned property subject to a maximum density of twenty-five (25) dwelling units per acre in accordance with the City Comprehensive Plan Future land Use Element. Townhouse development shall comply with the standards set forth in Section 32-731. Permitted density is subject to availability and apportionment of flexibility or reserve units under the Broward County Flexibility Rules document of the Broward County Land Use Plan. It is further provided that density of any such development shall be subject to approval by the City Commission and the City shall not be required to allow flexibility units to permit the maximum density prescribed by the Comprehensive Plan.



## foster road corridor overlay district

- c. Where residential dwellings are designed within the same project as non-residential uses, the developer shall be required to identify in a supplemental report and, on the face of plans, the special provisions made to ensure the compatibility of uses.
- d. Site plans and elevations shall show the typical architectural design of each side of all residential and commercial buildings, loading zone area, dumpster enclosure areas, and outside activity accessory areas; shall be submitted as part of the conditional use application material for the City Commission review and approval. Once approved, the development must be constructed in substantial compliance with approved site plans, elevations and other design plans.

### **Non-Conforming Uses**

- a. Any previously lawful use status made non-conforming by the provisions of this division shall be rendered null and void if the non-conforming use is abandoned or discontinued for a period of 90 consecutive days. In determining whether a non-conforming use has been abandoned or discontinued, the standards contained in Section 32-925 shall be applied.
- b. All property existing on January 2, 1979, which is not in compliance with the provisions of Section 32-384 Landscaping, and Section 32-453 Parking, shall be required to conform to the provision of said articles. However, where non-conforming existing development does not meet the minimum perimeter landscaping requirements of Section 32-384 said sections shall take priority over the provisions of Section 32-353 relative to parking.
- c. Non-conforming lots, structures, or uses located within this district, except as provided above, shall be subject to the provisions of Article IV of the Zoning and Land Development Code.



## foster road corridor overlay district

### **Prohibited Uses**

The following uses are prohibited within the Foster Road Overlay District:

- a. Liquor package stores
- b. Automobile parts and accessory uses
- c. Night clubs, dance halls and discotheques
- d. Alcoholic beverage establishments
- e. Automobile sales and displays
- f. Automobile rental agencies
- g. Boat sales and displays
- h. Bus terminals
- i. Car washes
- j. Taxi and limousine services
- k. Automobile repair and service activities
- l. Funeral supply and display stores
- m. Automobile paint and body shops
- n. Service stations with accessory gas sales
- o. Wholesale distributor showrooms
- p. Accessory manufacturing and retail sales
- q. Convenience stores with less than 2,000 square feet of gross floor area

### **Re-Development Area Modifications**

Section 32-177, applies within the Foster Road Overlay District, except as may be modified by the following:

- a. The City Commission may modify any Zoning and Land Development Code or Foster Road Overlay District regulation or requirement through the Redevelopment Area Modification process if it is determined to significantly inhibit redevelopment improvement efforts.
- b. The transmittal of a Redevelopment Area Modification Waiver Request for properties within the Foster Road Overlay District shall not be subject to the thirty (30) day deadline, but shall coincide with transmittal of the corresponding Major Development Review Application, if applicable.
- c. Administrative waivers: Within the redevelopment area modification area, staff shall be allowed to approve administrative waivers up to twenty (20) percent of minimum required as to number of parking spaces, landscaping, and setback requirements.

## foster road corridor overlay district

### Building and Site Design

Buildings or structures that are part of a present or future group or complex, shall have a unity of character and design. The relationship of forms and the use, texture, and color of materials shall be such as to create a harmonious whole site. Harmony can be achieved through the proper consideration of scale, proportions, site planning, landscaping, materials and color.

Buildings and site design relationships shall conform to the following criteria:

- a. Buildings or structures located along strips of land or on single sites and not a part of a unified multi-building complex shall strive to achieve visual harmony with the surroundings.
- b. No more than forty percent (40%) of the building façade on new buildings may be opaque.
- c. Establishments, which are located on corners, are recommended to place windows on each wall that faces a street, parking area or driveway.
- d. No more than forty percent (40%) of any façade, nor any area of more than 1,000 square feet should be blank wall without window or decoration treatments.
- e. Whenever feasible, off-street parking should not be located between buildings and urban streets.
- f. In the case of buildings with multiple storefronts, façade treatments shall be coordinated.
- g. All vending machines, or any facility dispensing merchandise or a service on private property shall be confined to a space built into the building or buildings or enclosed in a separate structure compatible with the main building.
- h. Site redevelopment: Where existing improvements are such that a boundary or edge is not provided by a building, such should include street treatments, such as street trees, Royal, Washingtonia, Date and Medjool Palms, or landscaped pergolas that are conducive to create a sense of boundary (street definition line) at a maximum of 10 feet from the front property line.
- i. Storefronts should have easily identifiable entrances.
- j. "Take out" or "pick up" windows for retail or other establishments should not be located on a building façade that faces a public right-of-way, unless they are designed in such a manner as to be an aesthetic asset to the building and neighborhood.



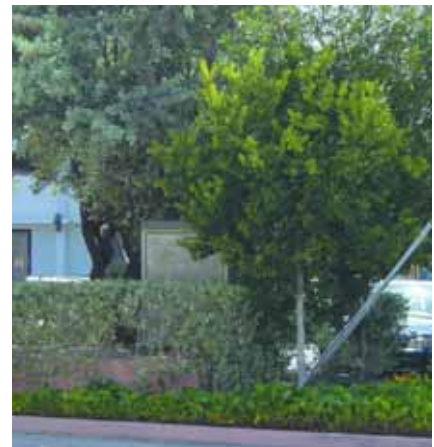
*"Proportions, color and materials in harmony"*



*"Street trees create a sense of boundary"*

## foster road corridor overlay district

- k. All roof-mounted equipment on all non-residential structures shall be screened by the use of a treatment, which is architecturally compatible with the rest of the structure. The screening shall effectively block the view and noise of roof-mounted equipment from the off-site locations.
- l. Every effort shall be made to place ground-mounted air conditioning units and other mechanical equipment to the rear of the building and screened from public and on-site pedestrian view.
- m. Wrought iron and aluminum bar fences shall either be black, white or match the color of the building. Masonry walls shall match the building color or reflect Florida or coastal themes. Color should be muted tones such as soft yellow, cream, granite, and rose.
- n. The gates shall not be chain link or wood, but must be aluminum or other durable material.
- o. Chain link or wood fences are prohibited in this district within commercially zoned properties, and any existing chain link or wood fences shall be removed.



*"Landscape effectively screens ground mounted equipment"*

### Streetscape Environment

Street furnishings are a very important element to a successful streetscape environment. Furnishings need to be attractive, pedestrian-friendly, durable and in harmony with the overall theme for which they are provided. Although street furnishings should be constructed of low maintenance materials, they should be implemented in conjunction with a maintenance strategy. The frequency and type of attentions allocated to these elements should be a critical factor in their selection. Street furniture may be located within a development or in the right-of-way or public use easement, such as bus shelters and newsracks.

The following criteria shall be considered:

- a. Newsracks within the District shall be uniform in color and adhere to the requirements of the Newsrack Ordinance #1998-29. Only modular newsracks are permitted within the City rights-of-way or private property, except that, where only one individual newsrack is located in the area, the Director may allow a single newsrack if deemed appropriate. All modular and single newsracks in the public right-of-ways shall be painted black. Newsracks on private property visible from public view shall also be painted black.
- b. Bus Shelters – Bus shelters within the District shall be uniform in material, style and color as established by the City.



*"Furnishings shall be made of low maintainance materials"*

## foster road corridor overlay district

### Landscape Buffers

Landscape treatment/improvements shall conform to applicable landscape requirements found in Article IX, Landscape Regulations. Landscaping includes not only trees and plantings, but potted plants and planters. Xeriscaping will be encouraged.

Landscaping may be used for screening purposes, which means the masking out or concealing of any objectionable area, such as parking lots, trash, service areas, or outside mechanical equipment. The following criteria shall also be considered:

- a. Parking lots, vehicular use areas, and their parked vehicles are to be effectively screened from public view and from adjacent property in a manner that is attractive and compatible with safety, the neighborhood, and facilities serviced.
- b. Off-site canopy street trees of a minimum of 20 feet in height at the time of planting of complementary species, shall be provided every 25 feet along public streets intersecting Foster Road abutting the development, whenever feasible.
- c. Landscaping of Existing Properties – Whenever an existing structure is altered in any way requiring approval of the present Design Guidelines criteria, such should be upgraded to meet those requirements, unless in the opinion of staff, such upgrading is impractical or would result in undue hardship.
- d. Pedestrian scale amenities are encouraged such as seating (benches/low walls/wide planter edges); brick and decorative concrete paving patterns; fountains and public art; planters; enhanced pedestrian crosswalks with brick or euro-tiling and wider sidewalks, sidewalk cafes, and courtyards and at entrance and exits into the developments. The pavers should be the same or similar to the pavers surrounding the development.
- e. Landscaping, decorative pavers, fencing, traffic control signage and lighting should be consistent and compliment that already included in District's streetscape.



*"Planters and potted plants are encouraged"*



*"Street trees shade and enhance the development"*



## foster road corridor overlay district

- f. Location of plants and planters should be carefully planned to define small, intimate outdoor space. The landscape should not hide windows or special architectural elements, nor should they crowd entrances or pedestrian traffic as well as they should not create hazards to pedestrians or restrict motor traffic. Planters shall be properly maintained and compliment other furnishings of the development.
- g. Building wall areas more than 20 feet in length shall be interrupted by landscaped areas and planted with a minimum of one tree per every 20 feet of linear wall area to break up the monotony of the wall face.

### Access Requirements and Off-Site Improvements

Off-site improvements associated with the new development or redevelopment as may be required by Article V, Development Review Requirements, shall be subject to the following:

- a. The number of curb cuts on the Foster Road Corridor shall be minimized during review of projects for new development or redevelopment projects.
- b. Street rights-of-way, unessential for the use of the public should be vacated when feasible.
- c. Off-site improvements shall be consistent with the city's established specifications for such improvement or subject to City Commission approval.
- d. Drainage and disposition of natural and storm water shall be designed to be compatible with existing systems.
- e. Proposed utility services shall be designed to be compatible with existing services, however, this shall not preclude the installation of services underground for new development. On site utility must be underground for new development or redevelopment.

### Signage

- a. Signage on buildings with multiple storefronts shall be of the same type and material, i.e.: channel letters illuminated, illuminated, box sign, or plastic letters, however, uniform color of such signs is not required to allow for diversity. Painted wood wall signs are prohibited in the Corridors.



## foster road corridor overlay district

### Off-Street Parking and Loading

Minimum off street parking and loading requirements shall conform to Article IV Division II Off Street Parking and Loading Requirements. Non-residential uses shall comply with the "Administrative Parking Standards Document."

The following criteria shall also be considered:

- a. Off street loading areas shall be located where they will not disturb adjacent uses and should not be the visual focal point of a driveway, parking area, adjacent properties, or the right-of-way. They may be accomplished by providing any or a combination of the following: masonry wall extensions of the building line, opaque landscape screening, berming, and through selective placement or orientation of the loading area.
- b. Mixed Commercial/Residential projects:
  - (1) Parking shall be calculated as required by Article IV of the code for each of the uses. Residential parking shall be calculated as provided in Section 32-455(b)(3). It shall be unlawful to park, store or keep any commercial vehicles between the hours of 9:00 P.M. and 6:00 A.M. the following morning or to park, store, or keep any boats, trailers, or recreation vehicles within the residential portion of any project.
  - (2) Parking spaces required for the commercial portion of any project shall not be employed for the parking of boats, boat trailers, or overnight parking of commercial equipment as defined in Section 28-6.



*"Trees and shrubs screen wall and loading areas"*



*"Selective parking placement will not impact neighbors"*

## foster road corridor overlay district

### Lighting

Lighting comes in two types: functional and ambient. Lighting, in addition to Florida Power and Light standards, shall be designed from the standpoint of safety and reviewed as an integral part of the overall development design.

- a. Parking lot fixtures are to be selected not only for their functional value, but also for their aesthetic qualities. They are to be considered furniture of the parking lot visible both day and night. Light fixtures used in the District shall be decorative for new development or redevelopment within public view and are encouraged throughout the development. The decorative fixtures shall be of a style that compliments the development. Cobra heads are prohibited within a development. Shoebox units may be used but are discouraged at entrances and exits.
- b. Ornamental street lamps should be installed, where feasible in a style similar to the fixtures already existing in the District.
- c. Parking lot and security lighting shall be designed to direct light into the property.
- d. The use of mounted building lights is discouraged, but if used, the light source should not be visible off site.
- e. The color of the light sources shall be consistent throughout the project. High-pressure sodium lamps are not permitted.



*"Lighting can be functional  
and aesthetic"*

## foster road corridor overlay district

### Awnings and Canopies

Overhead protection from rain and sun should be provided for pedestrians. Awnings also have an impact on the appearance of the storefront and building and to tend to bring pedestrians closer to shop windows and entrances.

Consideration shall be given to the following where applicable:

- a. Buildings/storefronts should have awnings or other means to provide pedestrians with sun/rain protection unless physically unsuited.
- b. Backlite awnings in the Foster Road Corridor Overlay District are strongly discouraged. These awnings, because of their high visibility, become attention-getting devices such as a sign, rather than a means to provide comfort and protection for the pedestrian. Such awnings overwhelm the appearance of buildings they are located on, detracting from architectural qualities. Awnings, which incorporate subtle down lighting in a manner, which creates a discreet peripheral washing of the awning, may be appropriate in some instances.
- c. Awnings shall be maintained in good repair, free from tears, fading or peeling. Awnings should be supported by poles, connected to the building underneath the awning. Awnings needing vertical support columns are prohibited in the setback area.



*"Awning create an inviting environment"*



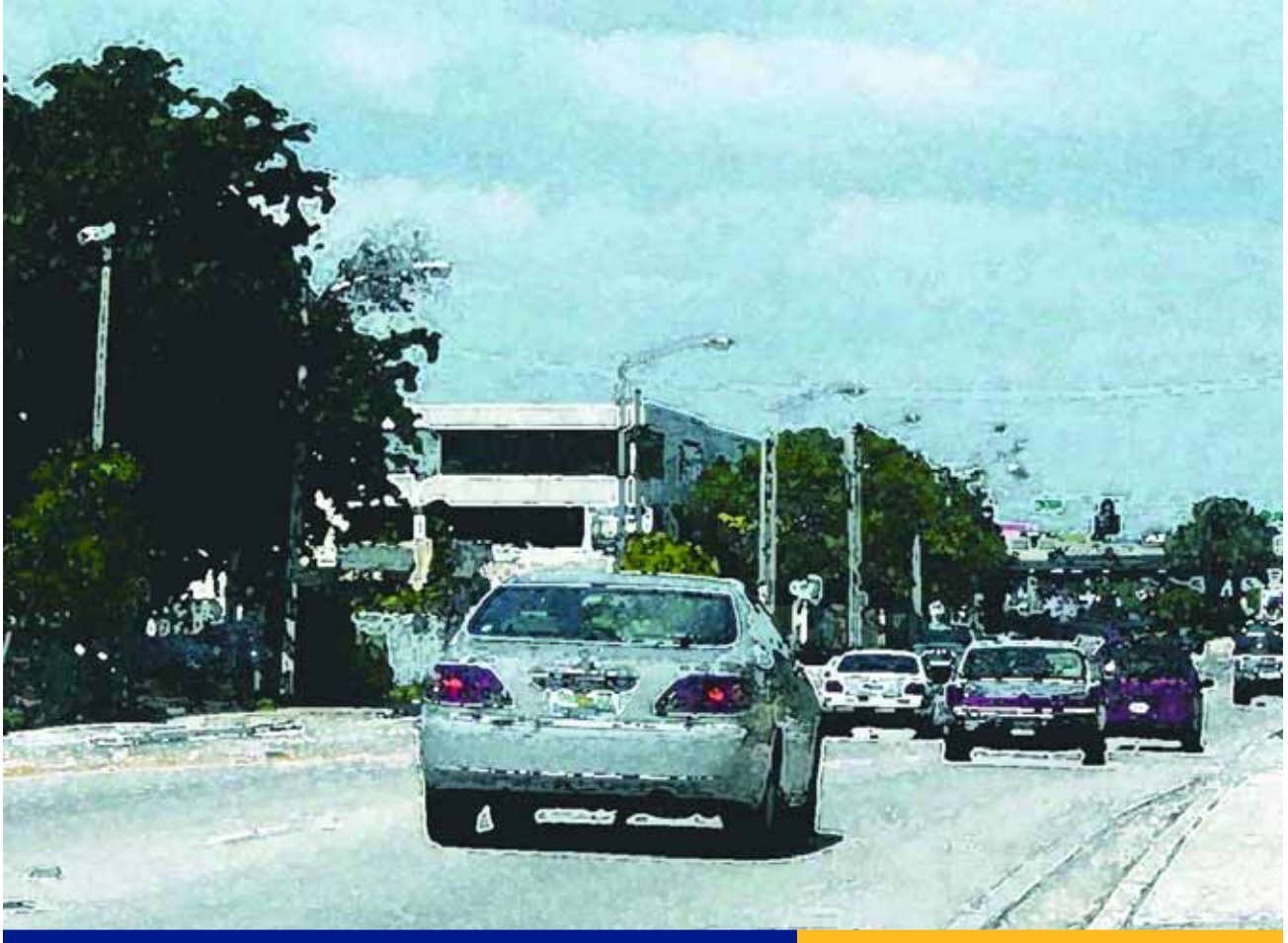
*"Awning shall be maintained in good repair"*



## pembroke road corridor overlay district

### **Section Eight**

8.0	Intention Statement	107
8.1	Pembroke Road Corridor Overlay District Map	108



## pembroke road corridor overlay district

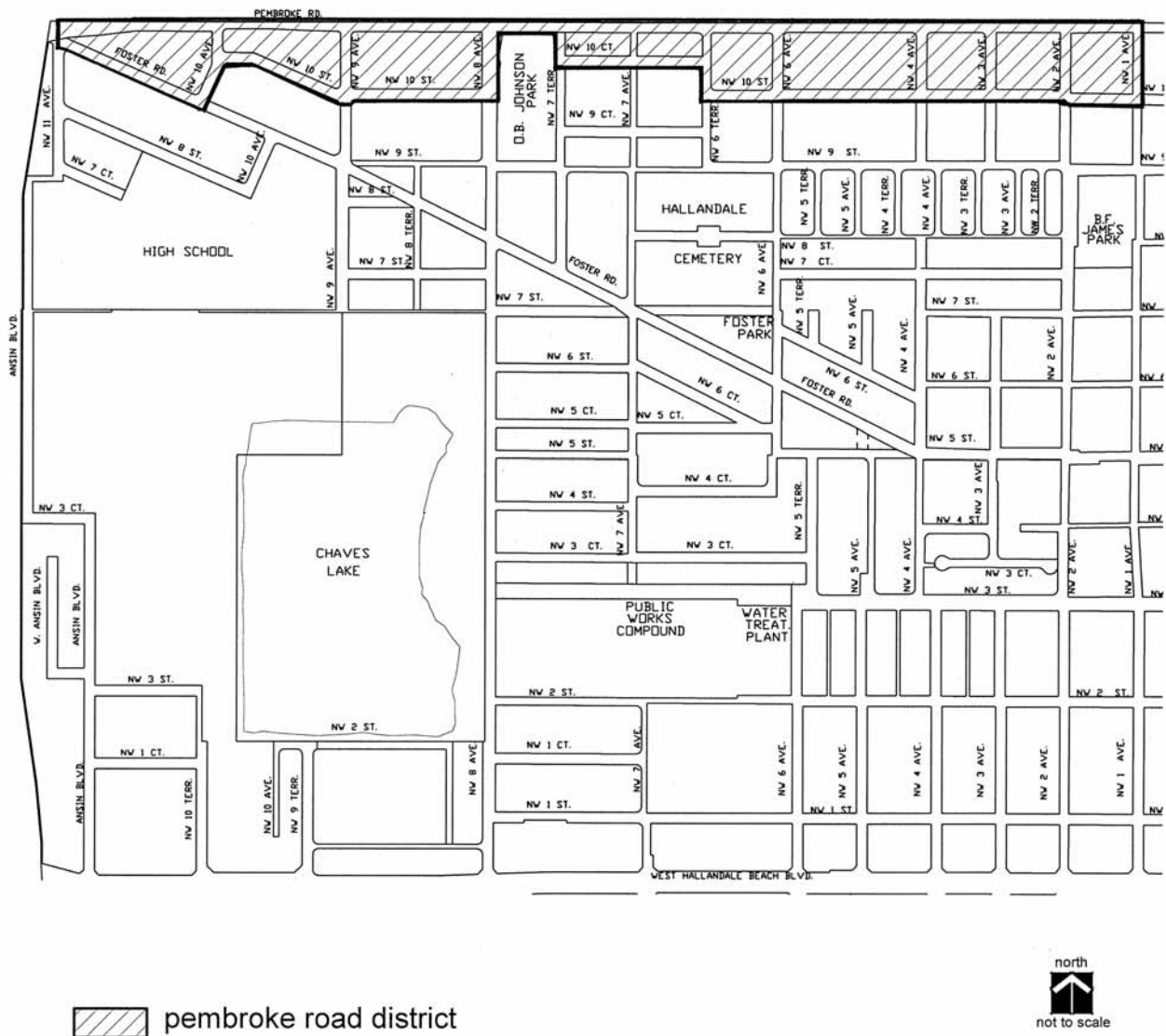
### 8.0 Intention Statement

The Pembroke Road Corridor Overlay District is a commercial corridor on the south side of Pembroke Road between Interstate 95 to the west and N.W. 8th Avenue to the east. The establishment of this district is intended to encourage redevelopment of the existing service/light industrial uses to service and retail businesses along this regional arterial roadway.

As the northern commercial edge of western Hallandale Beach, the planting of street trees is required for all development. Also, a landscape buffer and walls or opaque fences are required to screen all residential properties to the south.

New development should encourage an urban streetscape with building facades creating a harmonious blend of architecture in scale and character. The sharing of vehicular accessways between structures is encouraged to reduce the number of curb cuts. Parking lots should be accessed from the sides and rear and/or shared with adjacent properties wherever possible. Even though Pembroke Road is a major vehicular corridor, developers shall reinforce pedestrian connections.

## 8.1 pembroke road corridor overlay district map



## pembroke road corridor overlay district

### 8.2 Guidelines Applicable to Pembroke Road Corridor Overlay District

Ordinance No. 2001-24 of the City of Hallandale Beach Code of Ordinances has created an overlay district for the Pembroke Road Corridor, providing for district boundaries; establishing uses permitted in the district; providing for landscape buffer requirements; establishing access requirements; providing for street trees; mandating uniform signage and allowable fence types. It also creates a redevelopment area modification process, providing for conflict and severability.

#### **District Boundaries**

For the purpose of defining the boundaries, the Pembroke Road Overlay District is divided into two sectors: east and west of Johnson Park. The west sector is bounded by I-95 to the west; Foster Road and N.W. 10th Street to the south; N.W. 8th Avenue to the east and Pembroke Road to the north. The east sector is bounded by the N.W. 7th Avenue to the west; N.W. 10th Street to the south; N.W. 1st Avenue to the east; and Pembroke Road to the north.

All uses within the Pembroke Road Overlay District shall comply with the applicable regulations, the supplemental regulations of Ordinance 2001-24, and these design guidelines.

#### **Permitted Uses**

All uses specifically permitted by this provision and the underlying zoning district shall be permitted within the Pembroke Road Overlay District. Other uses, similar in nature to the uses permitted in the underlying zoning district but not specifically permitted therein, may be permitted within the Pembroke Road Overlay District if not prohibited by this provision.

Retail and service establishments limited to:

- a. Restaurants provided that:

Freestanding restaurants must have a minimum of 2,000 square feet of gross floor area.

- b. Service Stations, including convenience stores with accessory gas sales, provided that the business is not located within 1,500 feet of an existing service station or convenience store with accessory gas sales.



*"Landscape buffering creates an attractive streetscape in urban setting"*



## pembroke road corridor overlay district

### Prohibited Uses

The following uses are prohibited within the Pembroke Road Overlay District:

- a. Automobile Sales and Display (used)
- b. Automobile Repair and Service
- c. Boat Sales and Displays

### Landscape Buffers

The buffer area between a commercial use in the overlay district and a residential use separated by the right-of-way shall be at least ten (10) feet wide and contain dense landscaping capable of effectively screening the two uses. Screening shall be composed of landscaped masonry walls that are incorporated into the design scheme for the project. The buffer strip shall be provided along the exterior of the masonry wall. Screened walls shall be eight (8) feet in height and shall be subject to the vision clearance requirement of Chapter 32-332, Zoning and Land Development Code.

### Access Requirements

All development proposals shall provide shared access easements wherever feasible to adjoining properties and projects. Interconnection of developments shall be required as necessary to minimize the number of curb cuts and driveways onto local streets and particularly Pembroke Road and N.W. 10th Street. New or reconstructed access to commercial properties within the overlay district is restricted to Pembroke Road and no such access may be located a distance greater than 150 feet south of Pembroke Road along local streets. No new or reconstructed access is permitted to N.W. 10th Street, except for emergency vehicles.

### Signage

- (1) Signs are prohibited along N.W. 10th Street within the overlay district, except for traffic control, address signs and dedication plaques not exceeding three (3) square feet of sign area.
- (2) Uniform signage: Signage on buildings with multiple storefronts shall be of the same type (such as box sign, channel lettering, molded lettering). Painted wood wall signs are prohibited within the overlay district.



*"A masonry wall and landscape enhance architectural scheme"*



*"Uniform storefront signage"*

## pembroke road corridor overlay district

### **Walls and Fences**

- (1) An eight (8) foot high masonry wall must be provided with the required landscape strip. Such walls must be painted to blend in with the overall architectural scheme of the development proposal. All masonry walls must be finished on both sides so that no CBS courses are visible.
- (2) Barbed wire on fences and walls are prohibited in the overlay district.
- (3) Chain link fences facing Pembroke Road are prohibited. Existing chain link fences shall be replaced with allowable fencing within one year of adoption of this ordinance.

### **Landscape/Screening Treatment**

Building wall areas more than twenty (20) feet in length shall be screened by landscaped areas and planted with a minimum of one tree per every thirty (30) linear feet of wall area to break up the monotony of the wall face.

### **Provision of Street Trees**

The planting of street trees shall be required within the Pembroke Road Overlay District. Development proposals shall provide for installation of street trees in conformance with the following criteria:

- (1) Street trees shall not be counted toward the minimum number of on-site trees required by Section 32-384.
- (2) Street trees shall be a minimum of twenty (20) feet in height with a minimum diameter of three (3) inches at a point four (4) feet above ground level at time of planting and having
- (3) Trunks which can be maintained in a clean condition for over six (6) feet of clear wood measured from ground level.



*"Long building facade screened by landscape"*



*"Street trees are required in the district"*

## pembroke road corridor overlay district

- (4) Street trees shall be spaced, center to center, at a minimum of one (1) tree per twenty-five (25) linear feet of street frontage, exclusive of accessways.
- (5) The minimum height and specified street trees for major transportation corridor standards contained herein for street trees may be modified by the City Manager without penalty, in instances where the installation of the trees would interfere with overhead utilities, so long as the minimum number and spacing of trees is maintained.
- (6) Street trees shall be of non-invasive root species and shall be designated as street or shade trees in the City's approved plant material list. The designated street tree for Pembroke Road is the Royal Palm. The designation for all other local street trees shall be Mahogany trees.



*"Royal Palm is the designated street tree for Pembroke Road"*

### **Awnings**

Projecting canvas, fabric or metal awnings may be placed over doors or windows, but shall not project closer than two (2) feet to any property line. All awnings and awning signage installed in storefronts shall be maintained in good condition, free from tears, holes, fading or peeling. Repairs of awnings or awning signage shall be with the same or comparable materials.



*"Awnings on storefronts shall be maintained in good condition"*

## Section Nine

### 9.0 Code Authority

114





## 9.0 code authority

### Section One – Public Streetscape

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| 1.0  | Building and Site Design                                |                                 |
| 1.1  | Color Palette and Architectural Theme                   |                                 |
| 1.2  | Landscaping and Open Space                              | Section 32-382 to 32-385        |
| 1.3  | Off-Street Parking and Loading                          | Section 32-453 to 32-456        |
| 1.4  | Dumpster Enclosures, Garbage/Mechanical Equipment Rooms | Section 32-633                  |
| 1.5  | Awnings and Canopies                                    |                                 |
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| 1.7  | Pedestrian Protection                                   |                                 |
| 1.8  | Minimize Curb Cuts                                      |                                 |
| 1.9  | Streetscape Environment                                 |                                 |
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### Section Two – Planned Development District

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| 2.0 | Intention Statement              |                |
| 2.1 | Planned Development District Map |                |
| 2.2 | Administrative Process           | Section 32-173 |
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### Section Three – Planned Redevelopment Overlay District

Ordinance 2001-16

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| 3.0 | Intention Statement                |  |
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| 3.5 | Create Mixed-Use Development       |  |
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### Section Four – Fashion Row Overlay District

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| 4.0 | Intention Statement   |                |
| 4.1 | Fashion Row Overlay District Map                            |                |
| 4.2 | Guidelines Applicable to Fashion Row District               | Section 32-171 |
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| 4.4 | Guidelines Applicable to RM-18 Fashion Row Overlay District | Section 32-171 |

### Section Five – North Dixie Corridor Overlay District

- |     |  |                |
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| 5.0 | Intention Statement                                    |                |
| 5.1 | North Dixie Corridor Overlay District Map              |                |
| 5.2 | Guidelines Applicable to North Dixie Corridor District | Section 32-172 |

### Section Six – South Dixie Corridor Overlay District

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| 6.0 | Intention Statement                                    |                |
| 6.1 | South Dixie Corridor Overlay District Map              |                |
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### Section Seven – Foster Road Corridor Overlay District

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| 7.0 | Intention Statement                                    |  |
| 7.1 | Foster Road Corridor Overlay District Map              |  |
| 7.2 | Guidelines Applicable to Foster Road Corridor District |  |

### Section Eight – Pembroke Road Corridor Overlay District

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| 8.0 | Intention Statement                                       |                |
| 8.1 | Pembroke Road Corridor Overlay District Map               |                |
| 8.2 | Regulations Applicable to Pembroke Road Corridor District | Section 32-176 |